



**WILLEN
ROAD,
NEWPORT
PAGNELLE**

BLOOR HOMES



PREPARED BY PEGASUS URBAN DESIGN
ON BEHALF OF BLOOR HOMES

P19-2619_32C | OCTOBER 2021

ABOUT PEGASUS DESIGN

Pegasus Group's experience embraces all types of projects within the development industry from large scale urban extensions and strategies for area regeneration to renewable energy and residential schemes.

Our work supports planning applications and we have an exceptional understanding of the development control process and planning policy requirements in relation to design. Our design team is well regarded and is increasingly involved in expert design review and witness work.



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NOTE: THIS DOCUMENT IS DESIGNED TO BE VIEWED AS A3 DOUBLE SIDED



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WILLEN ROAD, NEWPORT PAGNELL



DESIGN OVERVIEW

The proposals in overview will provide:







- Up to 800 high-quality new homes
- New local centre, complementing the existing facilities in Newport Pagnell and the facilities proposed as part of the wider Milton Keynes East SUE
- New 2 form entry primary school
- 17 Ha of public open space, including a new riverside parkland, amenity green space and land for the provision of sport pitches
- New green link(s) and biodiversity and habitat enhancements
- Active and passive recreation including a local park with equipped areas for play, providing opportunities and spaces for adventure play across the development
- New vehicular access from Willen Road, and links to the wider SUE
- New overbridge over the A422 providing safe traffic free access to Newport Pagnell and Willen Road Sports Ground
- S106 and tariff based contributions



THE SITE

1.1 This statement has been prepared by Pegasus Design (part of Pegasus Group) on behalf of Bloor Homes and the wider consultant team, to accompany the Outline Planning Application for the residential development of Land east of Willen Road, Newport Pagnell, comprising:

“Outline planning application with all matters reserved except for means of access in relation to highway access from Willen Road and pedestrian/cycle access in relation to the crossing of the A422, for the demolition of the existing structures on site and the creation of a residential development of up to 800 dwellings comprising affordable housing, a primary school, local centre, public open space, red ways, sustainable urban drainage systems and all associated works.”

- KEY**
-  Site boundary
 -  SD12 MKE allocation boundary
 -  Existing PRow (subject to potential diversions)
 -  Existing MK Redway
 -  Existing leisure route/traffic free
 -  Existing quarry border

INTRODUCTION

1.2 This statement has been prepared in accordance with Article 9 of the Town and Country Planning (Development Management Procedure) (England) Order 2015 (DMPO), which requires certain applications to be accompanied by a Design and Access Statement. The DMPO also states the following requirements:

“(2) An application for planning permission to which this paragraph applies must, except where paragraph (4) applies, be accompanied by a statement (“a design and access statement”) about:

(a) the design principles and concepts that have been applied to the development; and

(b) how issues relating to access to the development have been dealt with.

(3) A design and access statement must:

(a) explain the design principles and concepts that have been applied to the development;

(b) demonstrate the steps taken to appraise the context of the development and how the design of the development takes that context into account;

(c) explain the policy adopted as to access, and how policies relating to access in relevant local development documents have been taken into account;

(d) state what, if any, consultation has been undertaken on issues relating to access to the development and what account has been taken of the outcome of any such consultation; and

(e) explain how specific issues which might affect access to the development have been addressed.”

PURPOSE OF THE STATEMENT

1.3 The purpose of this Design and Access Statement is “to explain how the proposed development is a suitable response to the site and its setting, and demonstrate that it can be adequately accessed by prospective users” (Para. 029, PPG, Reference ID: 14-029-20140306). This document achieves this within the following sections:

- **Section 1: Introduction.** Outlines the purpose of this document;
- **Section 2: Planning Policy.** Presentation of the key Planning Policy requirements, derived from a combination of Local Authority and National Government Policy;
- **Section 3 and 4: Context and Site Studies.** Considers the site and its surroundings in terms of the local physical, historical and social setting, as well as the technical and physical context;
- **Section 5: Developing the Design Concept.** Presentation of the design principles that have been derived from a combination of Government Policy and site assessment outlines key stakeholder engagement undertaken, as well as its key findings and design evolution;
- **Sections 6 to 15: Design Proposals.** Presentation of the key design proposals including the **Uses, Movement, Built form, Homes and buildings, Identity, Public Space, Nature, Resources and Lifespan**; and
- **Section 6: Conclusion.**

1.4 This Design and Access Statement has been written to respond to the Ministry of Housing, Communities and Local Government National Design Guide (NDG) ten characteristics of well-designed places. Highlighted items above are the ten characteristics of well-designed places, as set out in the National Design Guide.

1.5 This Design and Access Statement should be read in conjunction with the Outline Planning Application and its accompanying supporting documents.



THE SITE

- 1.6 The application extends to approximately 45.8 Hectares (Ha) and is located to the east of Willen Road, approximately 1.7 kilometres (km) to the south of Newport Pagnell town centre, and 4.8km north-east of central Milton Keynes
- 1.7 Forming part of Caldecote Farm the site comprises Caldecote Farm Quarry (no longer operational and in the process of being reinstated) and 8 no. predominantly arable fields, separated by low quality hedgerows, with a series of existing ditches located in the east of the site, flowing to the River Ouzel or Lovat, located to the east of the site. The A422 bisects the north of the site, separating an area of approximately 3.9 Ha from the main body of the site.
- 1.8 The boundaries of the site are predominantly defined by existing hedgerows with sporadic tree planting.
- The northern site boundary is defined by an existing hedgerow to the west, and an area of woodland planting to the east, beyond which the Willen Road Sport Ground, home to Newport Pagnell Town Football and Social Club, Newport Pagnell Tennis Club and Willen road skate park are located.
 - To the east the site boundary is formed the River Ouzel or Lovat.
 - The southern boundary is formed of a ditch and hedgerow to the east and bunding and earthworks to the west, screening Caldecote Farm Quarry from the travellers site located to the south-west of the site.
 - Willen Road forms the site western boundary and features mature tree planting to both sides of the street.
 - Both the northern and southern sides of the A422 the site boundary is defined by a post and rail fence and drainage ditch beyond.

- 1.9 The site is allocated for development as part of Policy SD12 Milton Keynes East Strategic Urban Extension, in the Milton Keynes Council (MKC) Local Plan (Plan:MK 2016-2031) adopted in March 2019.
- 1.10 There are 2 residential properties inset within the site boundary, Caldecote Cottage to the west and Moat Cottage to the east. The properties form part of the SD12 Allocations, however they are currently retained and not included in the site boundary for development, access to these properties will be retained within the development proposals.



VIEW WEST ACROSS THE SITE





VIEW NORTH ACROSS THE SITE, FROM THE SOUTH-EAST OF THE SITE AND THE PROW CROSSING THE SOUTHERN SITE BOUNDARY



VIEW TOWARDS WILLEN ROAD AND CALDECOTE FARM



VIEW NORTH-WEST ACROSS THE SITE TOWARDS THE A422



VIEW SOUTH-EAST ACROSS THE SITE TOWARDS MOAT COTTAGE



VIEW SOUTH FROM THE A422 TOWARDS THE MAIN SITE AREA AND CALDECOTE FARM

PLANNING POLICY

“Development that is not well designed should be refused, especially where it fails to reflect local design policies and government guidance on design, taking into account any local design guidance and supplementary planning documents such as design guides and codes. Conversely, significant weight should be given to:

a) development which reflects local design policies and government guidance on design, taking into account any local design guidance and supplementary planning documents such as design guides and codes; and/or

b) outstanding or innovative designs which promote high levels of sustainability, or help raise the standard of design more generally in an area, so long as they fit in with the overall form and layout of their surroundings.”

(Para. 134. NPPF 2021)

- 2.1 The development proposals will be formulated with due regard to the policies that make up the statutory Local Development Plan and Supplementary Planning Guidance, together with Government guidance contained within the National Planning Policy Framework (July 2021), National Design Guide (published in 2019 and updated in January 2021) and the National Model Design Code (January 2021).

NATIONAL PLANNING POLICY FRAMEWORK

- 2.2 Government guidance in the form of the National Planning Policy Framework (NPPF) sets out the Government’s planning policies and how these should be applied. The NPPF states at Paragraph 8 that the planning system has 3 interdependent key objectives, which when pursued in a mutually supportive way, can achieve sustainable development. The three key objectives are:

- An **economic** objective;
- A **social** objective; and
- An **environmental** objective.

- 2.3 There is a presumption in favour of sustainable development, as set out at Paragraph 11. Section 9: Promoting sustainable transport (para. 104) of the NPPF points to the role that design has to play in ensuring that transport issues are considered at the earliest stages of development proposals, and the role that design can play to ensure that development maximizes opportunities for sustainable transport options.

“...patterns of movement, streets, parking and other transport considerations are integral to the design of schemes, and contribute to making high quality places.”

(Para. 104(e) NPPF 2021)

- 2.4 The Government also continues to place a high emphasis on design and the NPPF expands on the principles of good design, to define what is expected of well-designed places. It also explains how policies and decision-making processes should support the inclusion of good design, providing detailed advice at Section 12: Achieving well-designed places. The contribution that good design makes to sustainable development is set out in paragraph 126, as follows:

“The creation of high quality, beautiful and sustainable buildings and places is fundamental to what the planning and development process should achieve. Good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development acceptable to communities...”

(Para. 126, NPPF 2021)

- 2.5 Furthermore, a new test is being introduced in the latest edition of the NPPF, to ensure that developments are well-designed, placing an emphasis on fostering of “beautiful” places among the overarching objectives of the planning system. In paragraph 134, the NPPF states that:

“Development that is not well designed should be refused, especially where it fails to reflect local design policies and government guidance on design, taking into account any local design guidance and supplementary planning documents such as design guides and codes”.

(Para 134, NPPF 2021)

- 2.6 The NPPF is also clear at paragraphs 127 and 128 that Development Plans should set out a clear design vision to provide certainty to applicants, and that design policies should be prepared in conjunction with local communities to reflect local aspirations.

- 2.7 Paragraph 130 of the NPPF states that with regard to design planning policy and decision making should ensure that developments;
- “a) will function well and add to the overall quality of the area, not just for the short term but over the lifetime of the development;*
- b) are visually attractive as a result of good architecture, layout and appropriate and effective landscaping;*
- c) are sympathetic to the local character and history, including the surrounding built environment and landscape setting, while not preventing or discouraging appropriate innovation or change (such as increased densities);*
- d) establish or maintain a strong sense of place, using the arrangement of streets, spaces, building types and materials to create attractive, welcoming and distinctive places to live, work and visit;*
- e) optimise the potential of the site to accommodate and sustain an appropriate amount and mix of development (including green and other public space) and support local facilities and transport networks; and*
- f) create places that are safe, inclusive and accessible and which promote health and well-being, with a high standard of amenity for existing and future users, and where crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion and resilience.”*

PLANNING PRACTICE GUIDANCE

- 2.8 The NPPF is accompanied by the on-line Government resource Planning Practice Guidance (PPG). Design: Process and Tools PPG provides guidance on the methods and processes available to both applicants and local authorities to ensure the delivery of well-designed and high-quality, long-lasting places with considered design solutions, under the following headings:
- Planning for well-designed places;
 - Making decisions about design;
 - Tools for assessing and improving design quality; and
 - Effective community engagement on design.
- 2.9 Paragraph 1 of the Design PPG reinforces the Government and NPPF’s commitment to requiring the creation of well-designed places and the role that early engagement can play in this.
- “Well-designed places can be achieved by taking a proactive and collaborative approach at all stages of the planning process, from policy and plan formulation through to the determination of planning applications and the post approval stage”*
- (para. 001, PPG, ID: 26-001-20191001, October 2019)*

NATIONAL DESIGN GUIDE

- 2.10 The National Design Guide (NDG) published by the Ministry of Housing, Communities and Local Government (MHCLG) in 2019 and updated in January 2021 further reinforces the way in which the design process can be used to ensure the delivery of quality places:
- “In a well-designed place, an integrated design process brings the ten characteristics together in a mutually supporting way. They interact to create an overall character of place.”*
- (Item 13, NDG 2021)*
- 2.11 The NDG outlines and illustrates the Governments priorities for well-designed place in the form of ten characteristics, based on national planning policy, planning guidance and objectives for good design.
- 2.12 The ten characteristics contribute towards the cross-discipline themes for good design set out in the NPPF and fall under three broad aims:
- To create physical character;
 - To help to nurture and sustain a sense of community; and
 - To positively addresses environmental issues affecting climate.
- 2.13 Whilst the NPPF, PPG and NDG are the primary points of reference, there are other well-regarded design guidance documents that are still relevant to creating good design including:
- **Manual for Streets 1 & 2** (Department of Transport/ Department for Communities and Local Government, 2007/2010); and
 - **Building for a Healthy Life** (Homes England, June 2020) is the latest edition, and new name for Building for Life 12 (BFL12) written in partnership with NHS England, NHS Improvement and MHCLG.

NATIONAL MODEL DESIGN CODE

- 2.14 The National Model Design Code (NMDC) was published in January 2021 by the Ministry of Housing, Communities and Local Government. The purpose of this document is to:

“... provide detailed guidance on the production of design codes, guides and policies to promote successful design. It expands on the ten characteristics of good design set out in the National Design Guide, which reflects the government’s priorities and provides a common overarching framework for design.”

(Para 1, National Design Code 2021)

- 2.15 The NMDC document draws upon the NPPF’s commitment to ensure that local planning authorities are utilising visual tools, such as design codes and guides, to inform development proposals, which will consequently provide a framework for creating high-quality places, with a consistent and high-quality standard of design.

OTHER SUPPLEMENTARY GUIDANCE

- 2.16 Whilst the NPPF, PPG and NDG are the primary points of reference, there are other well-regarded design guidance documents that are still relevant to creating good design including:
- Manual for Streets 1 & 2 (Department of Transport/ Department for Communities and Local Government, 2007/2010); and
 - Building for a Healthy Life (Homes England, July 2020) is the latest edition, and new name for Building for Life 12 (BFL12) written in partnership with NHS England, NHS Improvement and MHCLG.
 - Local Planning and Design Guidance
- 2.17 The development proposals have been formulated having due regard to the Milton Keynes Council (MKC) Local Development Plan comprising:
- MKC Plan:MK 2016-2031, adopted March 2019
 - MKC Site Allocations Plan, adopted July 2018
 - Minerals Local Plan, adopted July 2017
 - Waste Development Plan Document 2007-2026, adopted February 2008
 - Consideration has also been given to the following local planning and design guidance;
 - Newport Pagnell Modified Neighbourhood Plan, Made version June 2021
 - MKC New Residential Development Design Guide SPD, adopted April 2012
 - Milton Keynes East Strategic Urban Extension Development Framework, adopted 2019
 - Consultation Planning Obligations SPD, adopted 2019
 - MKC Affordable Housing SPD, adopted January 2020
 - MKC Parking Standards SPD, adopted January 2016
 - A Transport Vision and Strategy for Milton Keynes 2011-2031, adopted 2011

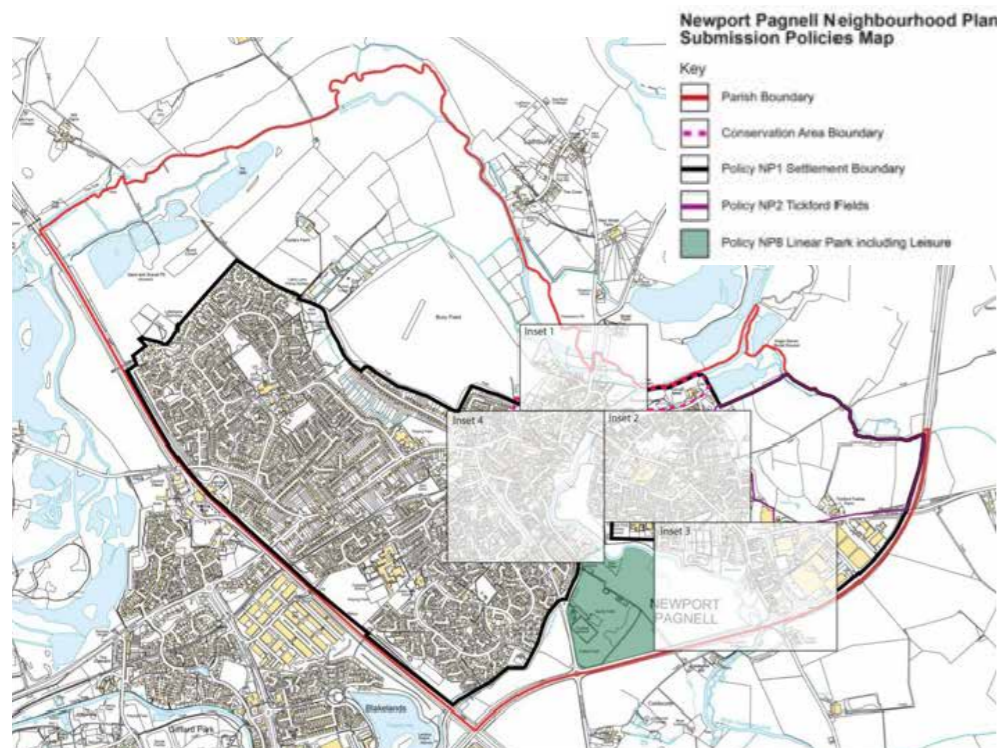
NEWPORT PAGNELL NEIGHBOURHOOD PLAN

- 2.18 The Newport Pagnell Neighbourhood Plan (NPNP) was adopted by Milton Keynes Council in May 2021 and will be in force until 2031. Plan:MK which was adopted in 2019 identifies three key settlements for the borough, and with Newport Pagnell being one of those listed, this area had to accommodate new houses to meet targets.
- 2.19 This document outlines the future growth and goals for the town until 203, protects further sites from development and ensures that any new development is executed well and with the support from the correct infrastructure. The Neighbourhood Plan also sets the historical background and current socioeconomic statistics for the town to support its vision.
- 2.20 The NPNP allocates the north of the site for recreational use, as set out on the NPNP Submission Policies Map below. The intention is for this area of land to provide further playing pitches, enhance the offering of Willen Road Sports Ground, instead of smaller scale piecemeal development across the town.



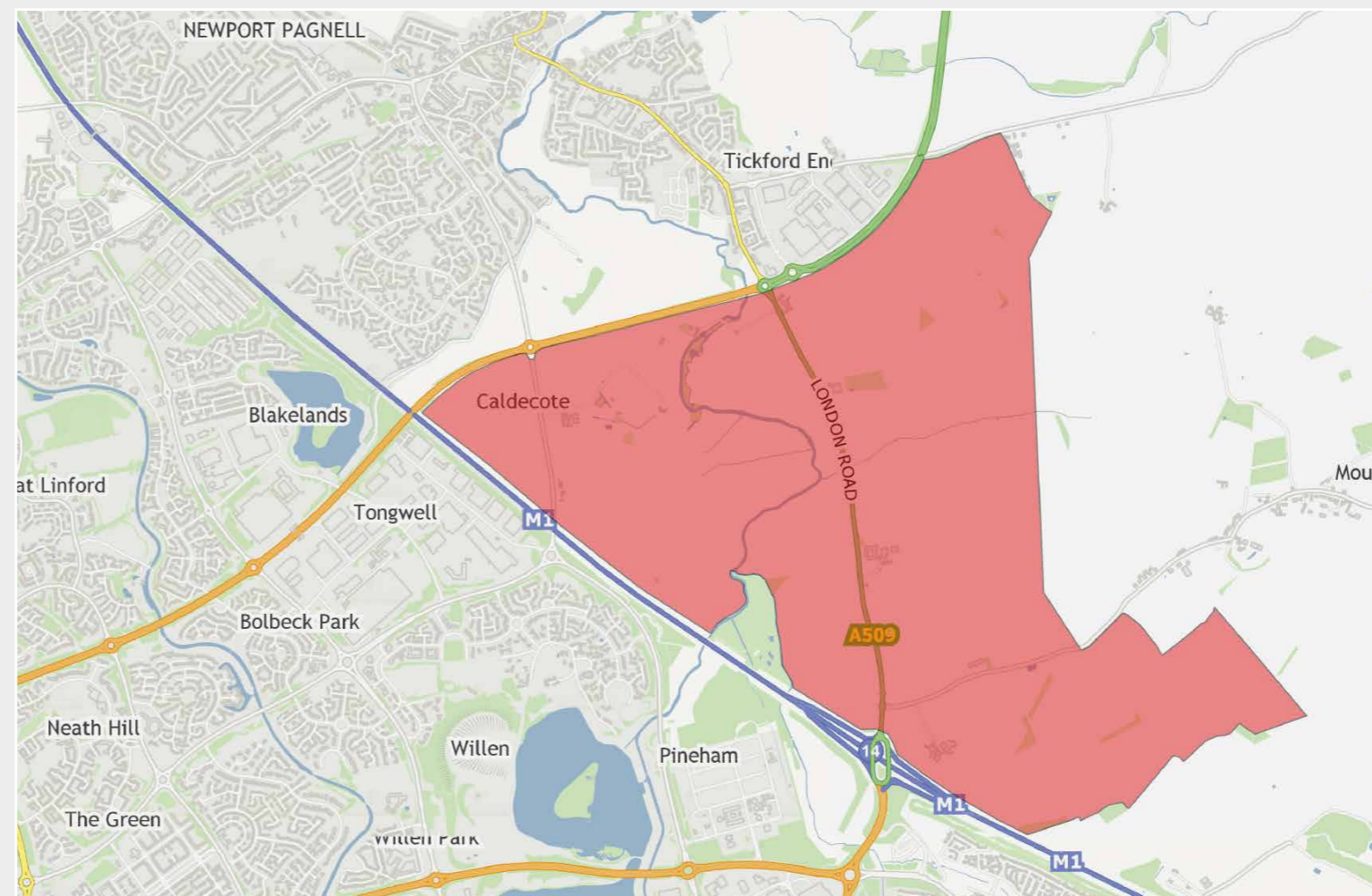
**POLICY NP8:
LINEAR PARK INCLUDING LEISURE**

The land to the East of Willen Road, as shown on the Policies Map, is designated as an extension to the linear park for recreational use, public access and nature conservation, to complete the link between Riverside Meadow and the proposed Ouzel Valley Park extension. Development proposals for enhanced recreation and sporting facilities within this area will be supported, subject to them not prejudicing the primary function of the linear park for outdoor and indoor recreation and leisure, landscape and nature conservation, and flood control.



**ALLOCATION SD12 - MILTON
KEYNES EAST STRATEGIC URBAN
EXTENSION**

- 2.21 The site is allocated for residential development/ residential-led mixed-use development under Policy SD12 “Milton Keynes East Strategic Urban Extension” in the Local Plan, Plan:MK 2016-2031, adopted March 2019.
- 2.22 Allocation SD12 relates to an area of 461 Ha and seeks to accommodate 5,000 dwellings as part of a residential led/mixed-use development. The application site (41 Ha) represents approximately 9% of the total allocation area.



STRATEGIC URBAN
EXTENSION: MILTON
KEYNES EAST
(PLAN:MK, MARCH
2019, MKC)

POLICY SD12 - MILTON KEYNES EAST STRATEGIC URBAN EXTENSION

- 2.23 Land is allocated at Milton Keynes East – as shown on the Key Diagram and Policies Map – for a comprehensive new residential and employment development to meet the long-term needs of Milton Keynes. Development can commence once the necessary strategic infrastructure required to make the site deliverable is funded and is being delivered. In that circumstance, the development of the site will be allowed to proceed within the plan period as an additional source of housing and employment land supply.
- 2.24 Development will be brought forward in line with all relevant policies in Plan:MK, particularly Policies SD1, SD9, SD10 and INF1. A comprehensive development framework for the site will be prepared in accordance with the Policies SD1, SD9, SD10 and INF1 and approved by the Council prior to planning permissions being granted.
- 2.25 The development framework and subsequent applications for planning permission will establish the quantum and form of development in more detail, but proposals for development will be expected to meeting the following criteria:
- Delivery of around 5,000 new homes, including at least 1,475 homes within the plan period, providing a range of sizes, types and tenures, including affordable housing, in accordance with other policies in the Plan.
 - Around 105 hectares of land for a mix of employment uses, complementing the role and function of CMK.

- Associated infrastructure including primary and secondary education, community facilities, health, retail and local services and a hotel. The development should comprise at least one district centre and/or local centre(s), of scale commensurate to the needs of the new community and that would not adversely affect the viability and vitality of Newport Pagnell district centre, with a co-location of key facilities.
- The phased introduction of a comprehensive network of transport infrastructure in line with the Local Investment Plan, to include grid road connections to H4/V11 to the west and improved highway connections to Newport Pagnell and Central Milton Keynes (CMK), including new and/or enhanced vehicular crossings of the M1, involving highway works on and off-site.
- A corridor of land safeguarded for a fast mass-transit system, and associated infrastructure, enabling connectivity to CMK and other key destinations. The width of the corridor should be sufficient to enable a range of possible transit solutions to come forward whilst also ensuring the efficient use of land for achieving the scale of development proposed within this policy.
- A network of segregated, and where appropriate grade-separated, new and enhanced footpaths, cycleways and bridleways (including redways) to connect to existing routes beyond the site, including provision of appropriate pedestrian and cyclist crossings of the A422 and suitable safe and attractive crossings of the M1 as appropriate.
- A strategic green infrastructure framework and network of green spaces to meet strategic and local requirements that follows the guidance in the Council's Landscape Character Assessment and Green Infrastructure Strategy to ensure ecological connectivity, protect the identity and character of nearby settlements and mitigate any significant impacts on the landscape in accordance with Policy NE5.
- The creation of a linear park through the site that broadly correlates with the River Ouzel floodplain and existing green infrastructure assets of value within and adjacent to it.
- Be informed by appropriate surveys of archaeology, built heritage and ecology with appropriate mitigation of impact as consistent with other policies of the Plan and the NPPF. An archaeological field study, including a Geophysical Survey, where appropriate following desk-based assessment, will be required to identify potential below ground archaeology. Where feasible, the Council will expect below ground archaeology to be kept in situ in preference to its removal.

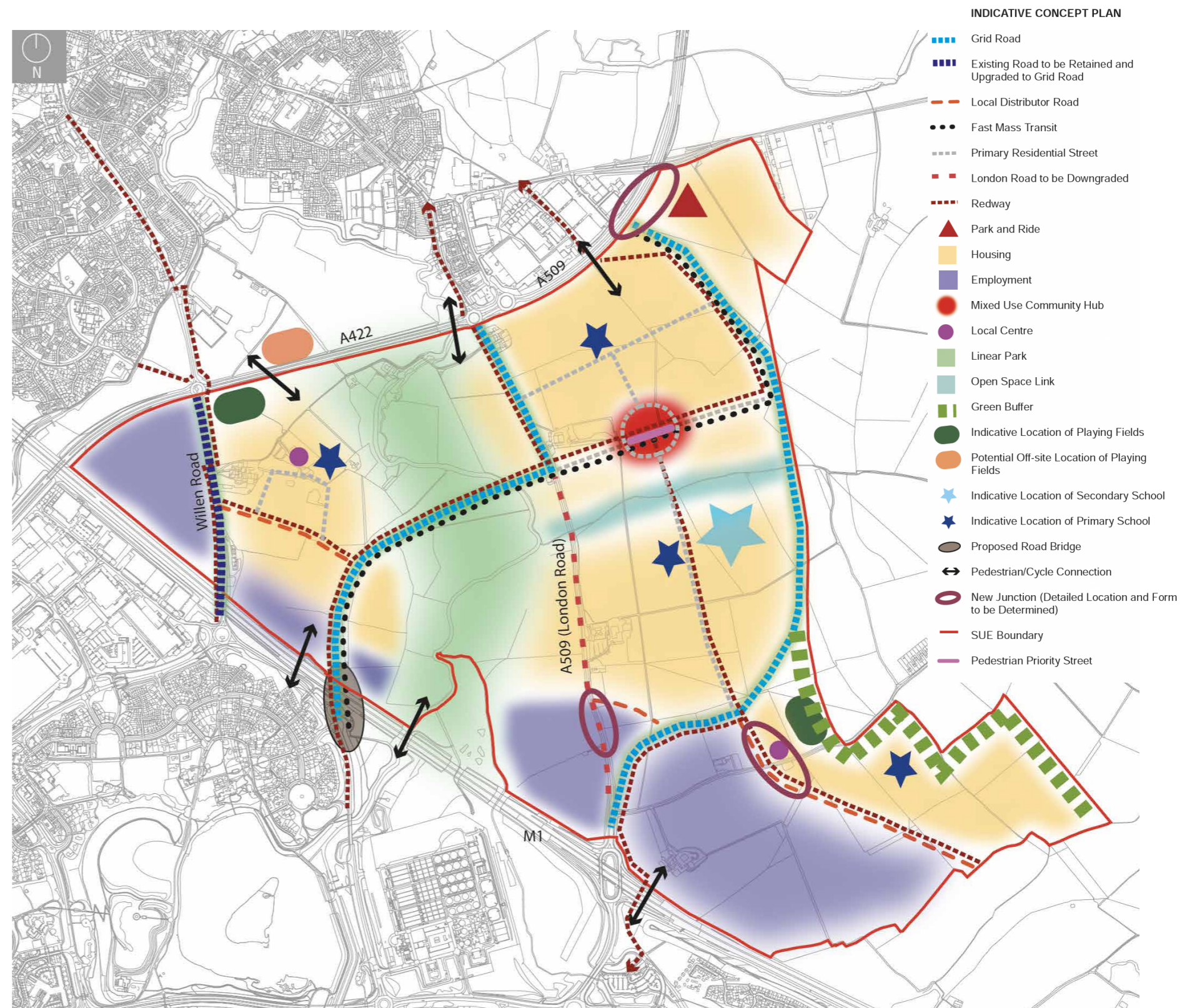


MILTON KEYNES EAST STRATEGIC URBAN EXTENSION DEVELOPMENT FRAMEWORK

- 2.26 The MK East SUE Development Framework (adopted March 2021) provides information on the East Strategic Urban Extension, which is expected to deliver 5,000 new homes, 105 Ha of employment land, a transport network and supporting green infrastructure.
- 2.27 Milton Keynes East is located to the east of the M1 and the South East of Newport Pagnell, the extension area for development is approximately 462 Ha.
- 2.28 Section 3 of the document outlines the vision, development principles and core concept for the proposed extension. The vision calls for a sustainable, high quality and thriving new community situated in Milton Keynes East. This will be supported by 12 design principles, including sustainable movement & rapid transport, quality placemaking, social & community and biodiversity.

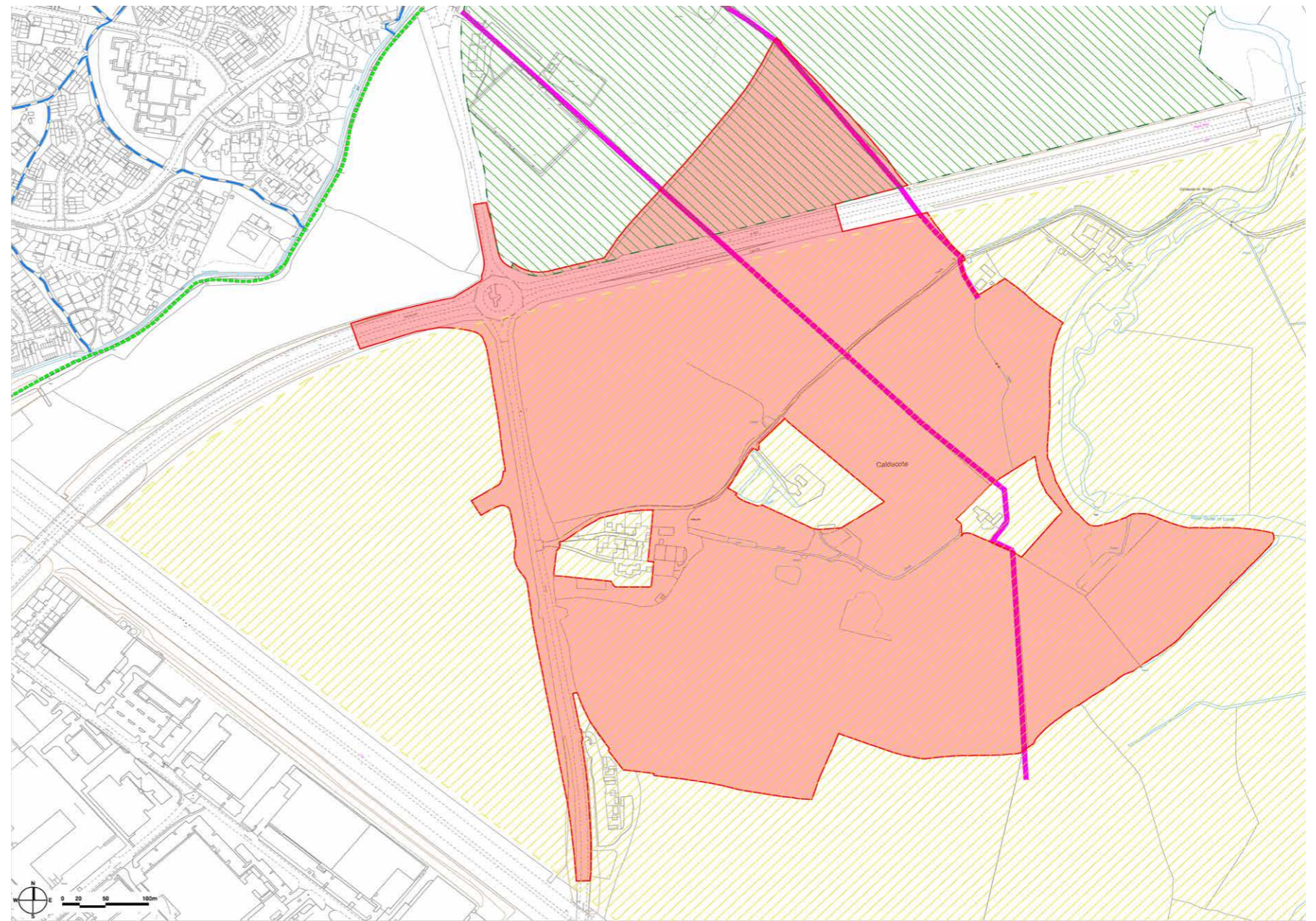
SITE EXTENTS

- 2.29 The proposed development will deliver the majority of the uses and features for this part of the SD12 allocation, as set out in the MK East SUE Development Framework SPD
- 2.30 The included elements of the MK East SUE SD12 allocation that are to be delivered as part of this planning applications are briefly described below:
- The proposals will deliver around half of the dwellings required in the plan period;
 - Employment will be provided within the development proposals, in line with the Councils framework, and will be delivered through the new local centre with a primary school
 - New vehicular access points to be provided from Willen Road (west of the site) and an improved junction arrangement is proposed between Willen Road and the Monks Way A422;



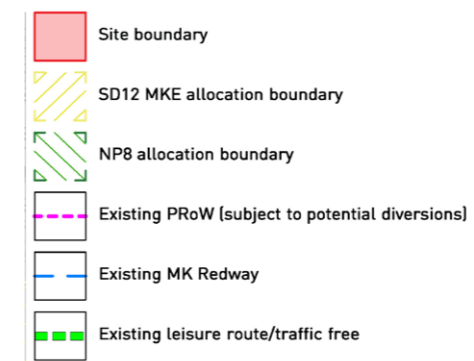
SPD CONCEPT PLAN

- The proposed development masterplan allows for a network of up to 4 new potential pedestrian and cyclist crossings of the A422 connecting northwards towards Newport Pagnell and the wider existing Redway network
- The proposals include space for a range of landscape spaces including but not limited to local park, pocket parks, local play area, linear park and allotments.
- The proposed development would not prevent the delivery of the remaining parts of the Policy SD12 allocation in the future, but this would be by others



SITE AND ALLOCATION BOUNDARY PLAN

	Minimum target provision (MK East Development Framework SPD)			On site provision (Ha)*
	Minimum size	Standard per 1,000 population (Ha)	Catchment area	
Local Park	1-2 Ha	not defined	300-400m	1.73
Pocket Parks	up to 1 Ha	not defined	300m	1.28
Local Play Area	0.2 Ha (0.35 Ha if surrounded by housing)	not defined	600m	0.67
Neighbourhood Play Area	0.6 Ha	not defined	400m	1.16
Amenity Open Space	up to 0.1 Ha	not defined	n/a	8.29
Playing Fields		6.4 Ha (para 4.2.26, pg 39)		3.38
Overbridge embankments	n/a	n/a	n/a	1.20
Total Public Open Space Provision				17.71

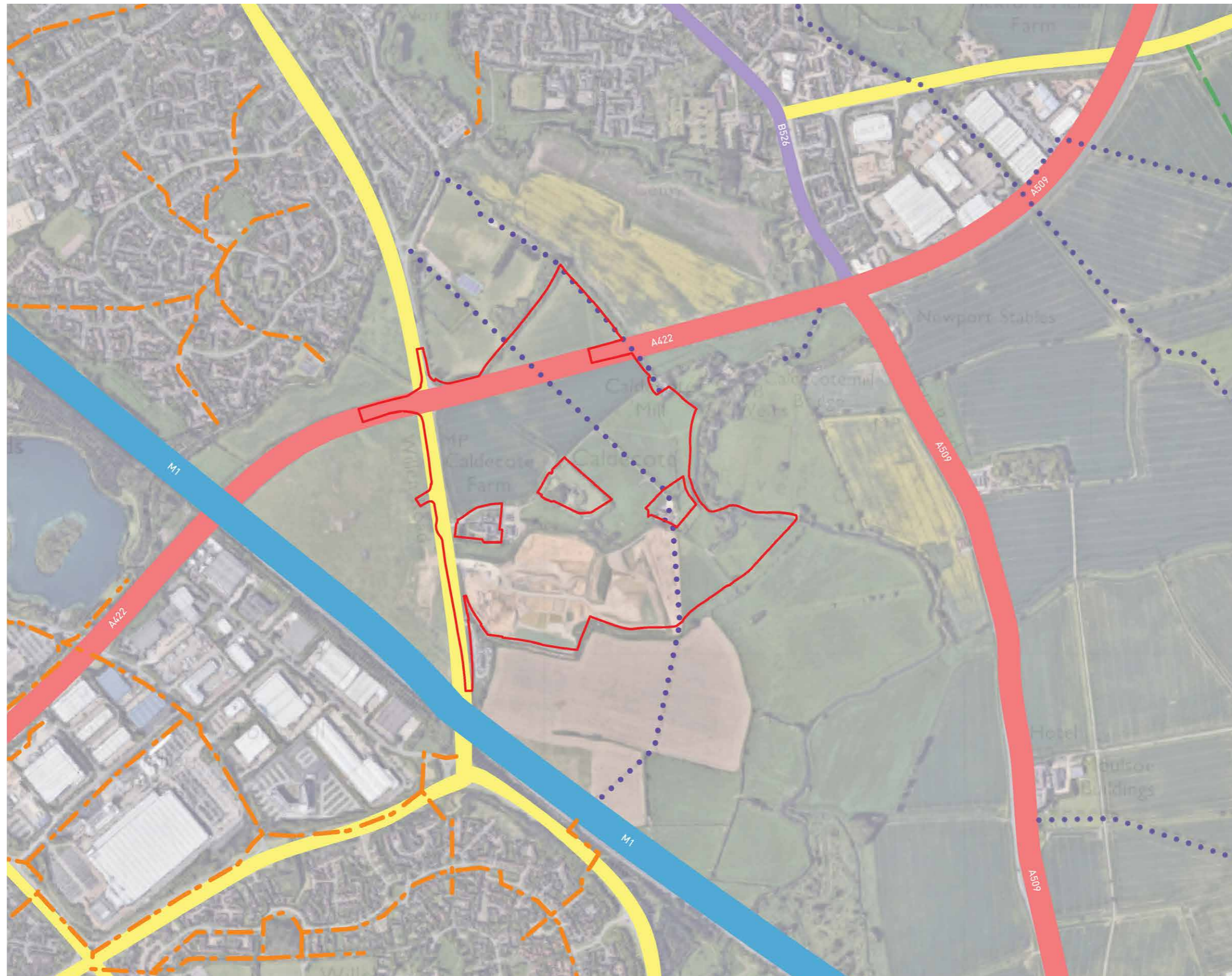


*All areas given include areas of existing tree/hedgerow planting, central SuDs corridor, attenuation basins, existing utilities and associated easements, and areas within flood zone 3.

PLANNING POLICY COMPLIANCE TABLE

2.31 The objectives and requirements of Policy SD12 have so far been met through:

Policy SD12	How the Masterplan has met Policy requirements
Delivery of around 5,000 new homes, including at least 1,475 homes within the plan period, providing a range of sizes, types and tenures, including affordable housing, in accordance with other policies in the Plan.	The proposals have the potential to deliver around circa 800 dwellings.
Around 105 hectares of land for a mix of employment uses, complementing the role and function of CMK.	Employment, in line with the Councils framework is envisaged adjacent to the motorway (i.e. to the west of Willen Road, outside this application site boundary) with aligned access points.
Associated infrastructure including primary and secondary education, community facilities, health, retail and local services and a hotel. The development should comprise at least one district and/or local centre(s), of a scale commensurate to the needs of the new community and that would not adversely affect the viability and vitality of Newport Pagnell district centre, with a co-location of key facilities.	A secondary local centre is provided as part of the proposals alongside (co-located) with a primary school site in accordance with the SUE Concept Plan.
The phased introduction of a comprehensive network of transport infrastructure in line with the Local Investment Plan, to include grid road connections to H4/V11 to the west and improved highway connections to Newport Pagnell and Central Milton Keynes (CMK), including new and/or enhanced vehicular crossings of the M1, involving highway works on and off-site.	New access junctions are provided off Willen Road and an improved junction arrangement is proposed between Willen Road and the Monks Way A422 (by RPS).
A corridor of land safeguarded for a fast mass-transit system, and associated infrastructure, enabling connectivity to CMK and other key destinations. The width of the corridor should be sufficient to enable a range of possible transit solutions to come forward, whilst also ensuring the efficient use of land for achieving the scale of development proposed within this policy.	The Framework Document shows the route for the Fast Mass Transit being to the south of the Bloor land control, so delivered off site as part of the larger scale development.
A network of segregated, and where appropriate grade-separated, new and enhanced footpaths, cycleways and bridleways (including Redways) to connect to existing routes, beyond the site, including provision of appropriate pedestrian and cyclist crossings of the A422 and suitable safe, and attractive crossings of the M1 as appropriate.	The Pegasus masterplan allows for a network of pedestrian and cyclist routes in and around the development. It specifically shows up to 3 potential pedestrian and cyclist crossings of the A422 connecting northwards towards Newport Pagnell and the wider existing Redway network (to the north west), the exact form of crossings will be subject to more detailed work in collaboration with highway engineers.
A strategic green infrastructure framework and network of green spaces to meet strategic and local requirements that follows guidance in the Council's Landscape Character Assessment and Green Infrastructure Strategy to ensure ecological connectivity, protect the identity and character of nearby settlements and mitigate any significant impacts on the landscape in accordance with Policy NE5.	The Pegasus Masterplan includes space for a range of landscape spaces including swales and attenuation areas alongside play and active and passive recreation and habitat creation areas. Current policy is lacking in guiding minimum quantum of open space typologies. The quantum of open space is to be discussed further with the LPA.
The creation of a linear park through the site that broadly correlates with the River Ouzel floodplain and existing green infrastructure assets of value within and adjacent to it. Be informed by appropriate surveys of archaeology, built heritage and ecology with appropriate mitigation of impact as consistent with other policies of the Plan and the NPPF. An archaeological field study, including a Geophysical Survey, where appropriate following desk-based assessment, will be required to identify potential below ground archaeology. Where feasible, the Council will expect below ground archaeology to be kept in situ in preference to its removal.	The development area abuts a length of the western side of the River Ouzel with built development set back to allow space for a significant linear park. (see above also) Work to be undertaken at the appropriate time.



KEY

- SITE
- MOTORWAY (M1)
- A ROADS (A422 AND A509)
- B ROADS (B526)
- MAIN ROADS
- PUBLIC FOOTPATH
- REDWAY
- PUBLIC BRIDLEWAY

MOVEMENT PLAN

CONTEXT

National Planning Policy Framework chapters 8, 12, 14, 15, 16

“An understanding of the context, history and the cultural characteristics of a site, neighbourhood and region influences the location, siting and design of new developments. It means they are well grounded in their locality and more likely to be acceptable to existing communities. Creating a positive sense of place helps to foster a sense of belonging and contributes to well-being, inclusion and community cohesion.”

(Para. 39, NDG 2021)

- 3.1 This section provides a summary of the assessment of the site and its surroundings that has been undertaken.

STREET PATTERN AND CONNECTIVITY

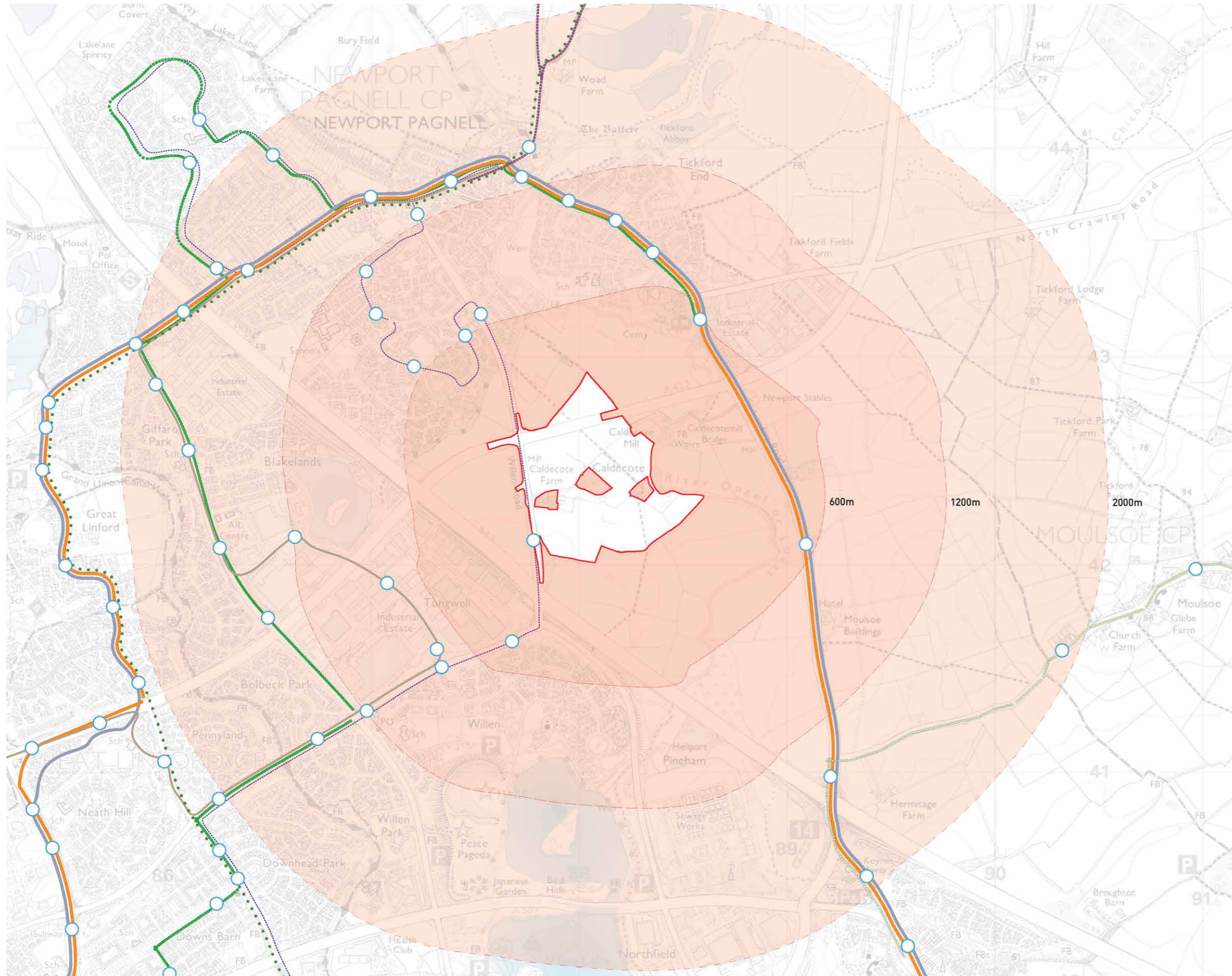
- 3.2 The site is well connected to the surrounding urban area of Newport Pagnell, and benefits from easy access to public transport and strategic highway links within close proximity of the site. The plan opposite illustrates the location of the site within the context of the local access and movement network.

PUBLIC TRANSPORT

- 3.3 The Number 1 bus route runs through the site, along Willen Road which connects to other residential suburbs and Milton Keynes Central. The Number 2 bus runs slightly outside of the site boundary which also connects to Milton Keynes Central. These services are both frequent, running every 30 minutes.
- 3.4 The closest train station to the site is Milton Keynes Central, which has links to London, Manchester and Birmingham. In addition, Milton Keynes Coachway Interchange is located 3km South of Willen Road, close to junction 14 on the M1.
- 3.5 London Luton Airport is approximately a 30-minute drive from the site.

PEDESTRIAN AND CYCLE CONNECTIONS

- 3.6 Local pedestrian and cyclist connections, include;
- Two public footpaths connect north from the site into Newport Pagnell, but the dual carriageway A422/A509 is a barrier to crossing
 - There are a network of public footpaths outside of the site within the open countryside around Moulsoe. There are opportunities for the SUE to connect into this network
 - In addition to public rights of way, there is a network of footpaths within Newport Pagnell that can connect into the SUE.
 - Within the linear parks, there are a network of leisure routes
 - Sustrans National Cycle Network Route 51 runs South of the site boundary and connects Oxfordshire, Cambridgeshire, Suffolk and Essex
 - Santander Cycles in Milton Keynes, with a docking station in Willen, is located approximately 1.6km of the Western site boundary
 - Newport Pagnell lies along the Redway Super Route which is suitable for longer commuter journeys on bike or foot to Milton Keynes.



KEY

- SITE
- BUS STOP
- MOTORWAY (M1)
- A ROADS (A422 AND A509)
- B526 ROADS
- MAIN ROADS

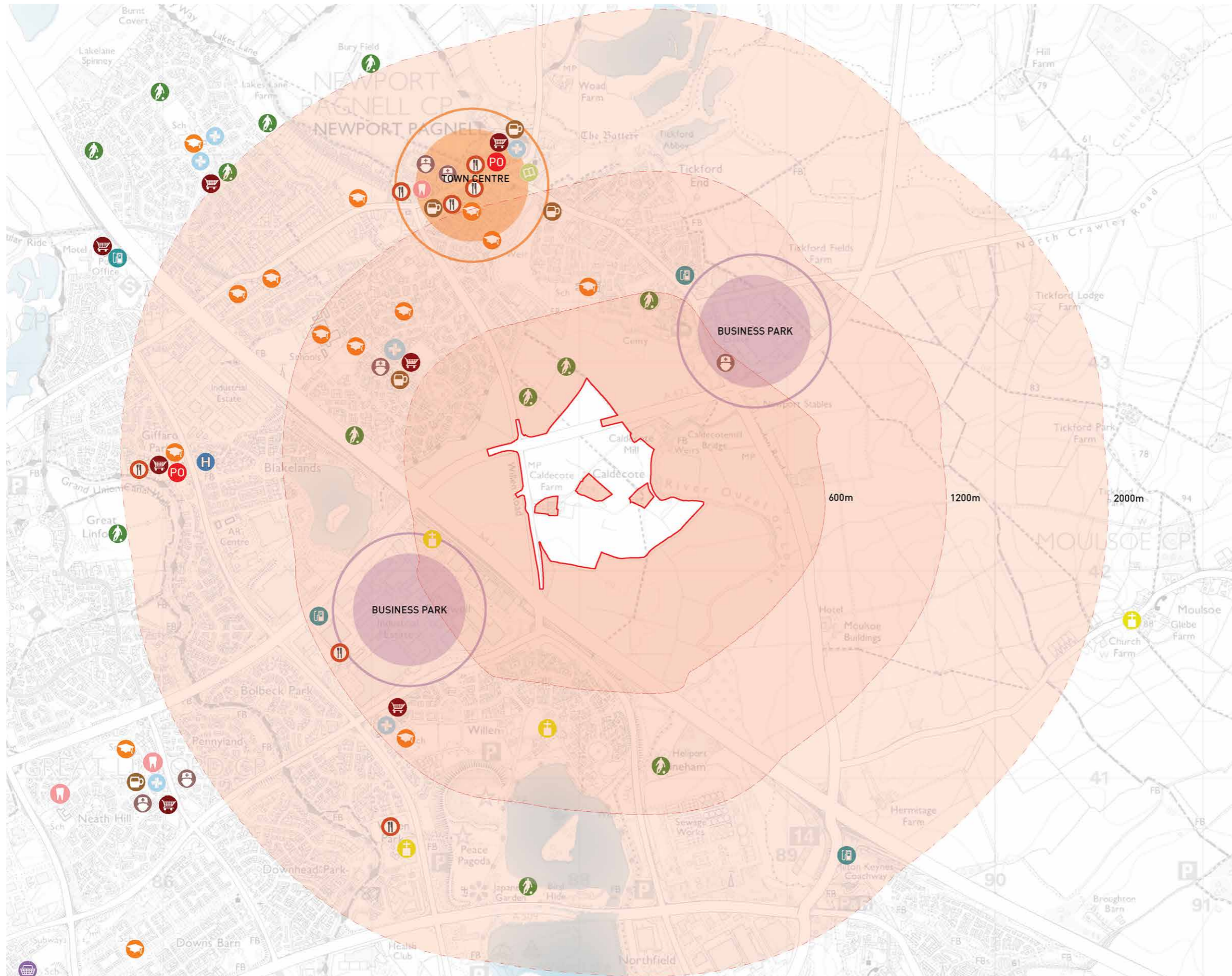
BUS ROUTES

- 1
- 2
- 21
- 24
- 25
- 30
- 37
- C10

TRANSPORT PLAN

EXISTING HIGHWAYS NETWORK

- 3.7 The site is directly accessed from Willen Road which runs in a North/South direction immediately West of the site. The route links to the A422/H3 Monks Way. To the North, Willen Road connects to the Caldecote Street and to the South: Tongwell Street; Danstead Way and Michigan Drive via a roundabout.
- 3.8 The A422 and Willen Road meet at Marsh End roundabout, immediately north-west of the site, providing links west to Milton Keynes and north to Newport Pagnell. From Marsh End roundabout going west the A422 is part of the MK grid road system, forming H3 Monks Way.
- 3.9 The M1 is located to the south-west of the site and is accessible at Junction 14 (approximately 3.5 km south-east of the site) and provides connections to Northampton, Daventry and Rugby to the north, and Luton and London to the south as well as the wider strategic highways network.
- 3.10 The site has links to major transport links to London, Leicester, Nottingham, Northampton, Sheffield and Leeds via the A422 and M1. The site is also located approximately 1.75km from Newport Pagnell Town Centre and 5.6km from Milton Keynes Central.



KEY

-  SITE LOCATION
-  PHARMACY
-  PETROL STATION
-  LIBRARY
-  HOSPITAL
-  PARK/RECREATION
-  PLACE OF WORSHIP
-  RESTAURANT/EATERY
-  POST OFFICE
-  SCHOOL
-  PUBLIC HOUSE
-  SUPERMARKET
-  CONVENIENCE STORE
-  DENTIST
-  DOCTORS SURGERY

EXISTING LOCAL FACILITIES PLAN

LOCAL FACILITIES

- 3.11 The Existing Local Facilities Plan (presented opposite) shows the location of key local facilities and services in relation to the site.
- 3.12 The site benefits from good accessibility to a range of existing community facilities. Newport Pagnell town centre is 1.6km (approximately a 20-minute walk) to the north of the site and provides a range of local shops and services, including cafes/restaurants, public houses, supermarkets, churches, doctor's surgeries, car parks and banks. Further retail options are also available in central Milton Keynes, 4.8km south-west of the site (approximately a 15 minute cycle).

RECREATION

- 3.13 Formal play facilities are provided at Syon Gardens and Elthorne Way, approximately 1 km north-east of the site, with further facilities provided at Burgess Play Park, approximately 1.8 km (a 22-minute walk) north-west of the site. Other facilities close to the site are:
- Willen Lake, Great Linford Park and Campbell Park
 - Stanton Low Country Park and Ouse Valley Park
 - Willen Football Club, Newport Pagnell Tennis Club, Newport Pagnell Town Football and Social Club.
 - Green Park Community Centre and Middleton Centre are both located approximately 1.2 km north-east of the site, and are a 15 minute walk from the northern site boundary;

EDUCATION

- Northern Pastures Pre-School (2.4km)
- Primary education facilities are available at Willen Primary School, approximately 1.6km from the Western site boundary.
- Tickford Park Primary School is also located within approximately 1.5km of the site; and
- Secondary provision is provided at Green Park School and Ousedale School Newport Pagnell, both approximately 1.6km of the site.
- Northern Pastures Pre-School (2.4km)
- Further education facilities are available at the University of Bedfordshire, which is located approximately 5.3km from the site

EMPLOYMENT

- Tongwell Industrial Estate (965m)
- Interchange Park (1.6km)

HEALTH

- Newport Medical Centre
- Willow Veterinary Centre (1.1km)
- Willow House Dental Practice, Newport Pagnell Dental Practice and Buckingham Priority Dental Service all within 1.6km of Northern site boundary
- Lloyds Pharmacy within 1.3km North West of site boundary
- Blakelands Hospital (2.6km)

HISTORICAL GROWTH

- 3.14 The town of Newport Pagnell grew as an important stopping point along the line of the Roman Fosse Way, linking Lincoln to Exeter. The area of Newport Pagnell has evidence of inhabitation since before the Iron Age. Bury Field still contains traces of earthworks from the civil war. Bury Field is an area of common land situated to the north of the site, which was used as defence during the Civil War, later for growing crops in the 20th Century and then in the 1960's as a sports field.
- 3.15 During the 13th Century, Newport Pagnell had numerous royal visits and was occupied by the Royalists during the Civil War. By the 18th century, the town was the centre of the extensive lace making industry, due to its importance as a transport route, connecting trade to Leicester, London, Cambridge and Oxford. In 1814, the Grand Junction Canal at Great Linford was made accessible, however use decreased when the railway link with Wolverhampton was created in its place 52 years later.
- 3.16 There are 2 significant bridges in Newport Pagnell, the North Bridge was constructed in 1810 and Tickford Iron Bridge in 1820. The North Bridge was originally constructed out of timber and then later rebuilt in stone with 3 arches. The Tickford Iron bridge is a listed bridge which was also completed in 1810 and is one of 3 still standing cast iron bridges in the world cast by Walker and Co.

- 3.17 Very little is known about the castle which belonged to the Paynels and Somerys, who founded the Parish, but a mount can be seen in the churchyard near the junction of the Lovat and Ouse.
- 3.18 In the 1950's, there was a surge in the town's population and large housing developments were constructed to house this new post-war population. When combined with the creation of the M1 in 1959 and the city of Milton Keynes, the population of Newport Pagnell soon surpassed 16,000.

“When determining how a site may be developed, it is important to understand the history of how the place has evolved. The local sense of place and identity are shaped by local history, culture and heritage, and how these have influenced the built environment and wider landscape.”

(Para. 46, NDG 2021)

LOCAL CHARACTER

“Local identity is made up of typical characteristics such as the pattern of housing, and special features that are distinct from their surroundings. These special features can be distinguished by their uses and activity, their social and cultural importance, and/or their physical form and design. Most places have some positive elements of character, particularly for their users. These can help to inform the character of a new development.”

(Para. 52, NDG 2021)

- 3.19 The National Design Guide states that well-designed new development is influenced by:
- “...an appreciation and understanding of vernacular, local or regional character, including existing built form, landscape and local architectural precedents;”***
- (Para. 53, NDG 2021)***
- 3.20 An analysis of the existing built form of Newport Pagnell can help identify patterns of development and key design components. Together these character generators and design components can help to inform the design approach.
- 3.21 The immediate site context is predominantly residential, where a range of architectural styles, detailing, materials and thereby character is evident, as demonstrated across the following pages.

3.22 Five character areas have been chosen to study as each area illustrates a morphological expansion of the town. With contrasting urban forms and building details each area provides a palette of design references that may be drawn from, allowing the proposed design response to reflect local character.

3.23 The six areas analysed are:

- ◆ Willen
- ◆ Newport Pagnell (Burgess Gardens)
- ◆ Newport Pagnell (Queens Avenue)
- ◆ Monkston
- ◆ Tickford End (Severn Drive)
- ◆ Hobart Crescent

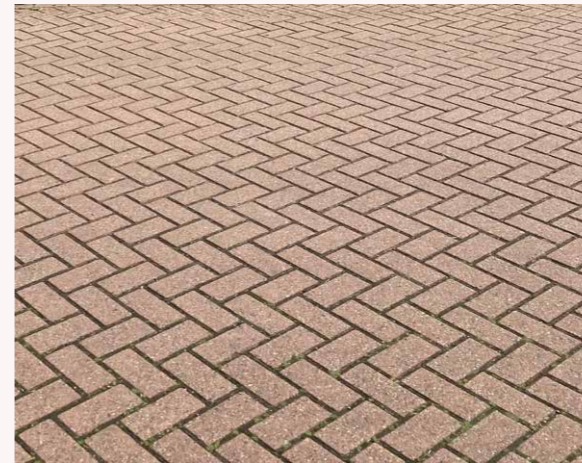
3.24 Each area is identified on the plan opposite and accompanying photographs across the following pages.



EXISTING LOCAL CHARACTER AREAS PLAN

WILLEN ALDRICH DRIVE

- 3.25 The area of Willen is within the immediate context of the site, just to the South-west of the M1. It is a largely residential area with its own local centre, coach station and primary school. The streets are predominantly well-lit and often tree lined with generous footpaths. Residents have good access to public open space and children's play areas, with Willen Lake situated just South of the main residential development. The building types consist of larger detached dwellings and occasional semi-detached which are predominantly 2 storeys with occasional 2.5 storey.
- 3.26 There is a mixture of loops and cul-de-sacs with houses that are set back from the road and parking is generally in view. Most dwellings benefit from private drives where there is sufficient space, however, there is also some informal visitor street parking. The dwellings have mainly been built in the last 30 years.
- 3.27 The material palette for this development largely consists of brick with occasional render and tile roofing. Detailing has been emphasised with chimneys, occasional gables fronting the street as well as defined entrances typically with pitched roof or flat front canopies. Strong building lines are created by detached and some semi-detached units, usually accompanied by informal boundary treatments with the use of lawns and some low-lying planting and occasional open spaces at intervals along the street.



LAYOUT

Form	Street Corridor	Building Set-back	Parking
There is a mixture of loops and cul-de-sacs with houses that are set back from the road and parking is generally in view.	The streets are predominantly well-lit and often tree lined with generous footpaths.	Strong building lines are created by detached and some semi-detached units, usually accompanied by open spaces at intervals along the street.	Most dwellings benefit from private drives where there is sufficient space, however, there is also some informal visitor street parking.

LANDSCAPE

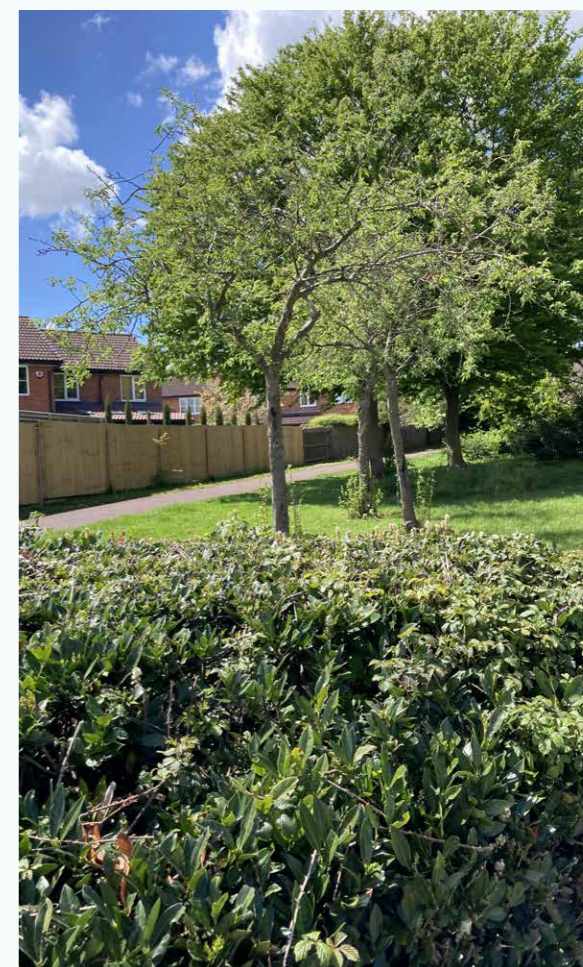
Public Space Typology	Planting	Boundary Treatment
Residents have good access to public open space and children's play areas, with Willen Lake situated just South of the main residential development.	Most streets are lined with trees, in addition to consistent intervals of large, planted areas. Primary streets also make use of green verges.	Boundary treatments are largely informal, with occasional hedges, low lying shrubs and wooden fences.

ARCHITECTURE

Building Heights	Building Types	Roof Materials	Wall Materials	Fenestration
Predominantly 2 storeys with occasional 2.5 storey.	The building types consist of larger detached dwellings and occasional semi-detached.	Roof tiles have been used and detailing has been emphasised with chimneys, gables fronting the street occasionally as well as defined entrances typically with pitched roof or flat front canopies.	The material palette for this development largely consists of brick with occasional render.	Often there is a horizontal window on the ground floor and then 2 vertical windows, sometimes with a small bathroom window on the first floor. Bay windows are used on occasion, with most windows either white or brown.

NEWPORT PAGNELL BURGESS GARDENS

- 3.28 Southwest of Newport Pagnell's high street, this area is predominantly made up of detached and semi-detached dwellings with the occasional row of terraced. The building heights are in keeping with the surrounding context of 2 storey with occasional 2.5 storey. The building materials broadly represent those of the local area, comprising of red brick with occasional render and mock Tudor detailing, tile roofing is also used throughout. The architectural style around this development reflects those of broadly the same era, with many of the dwellings built in the late 1990's.
- 3.29 Canopies and porches have been used to clearly define the front entrances and gables fronting the street occasionally add an asymmetrical interest. The boundary treatments are predominantly informal with lawns and low shrubs and planting to define the public and private spaces. Parking is generally in front or at the side of dwellings, with many houses benefiting from a private drive.



LAYOUT

Form	Street Corridor	Building Set-back	Parking
Houses have mainly been positioned in rows back-to-back, with many on tertiary streets in clusters of approximately 8-12 dwellings.	The streets are predominantly used by cars with generous footpaths.	The houses have a regular setback from street with small lawns.	Parking is generally in front or at the side of dwellings, with many houses benefiting from a private drive.

LANDSCAPE

Public Space Typology	Planting	Boundary Treatment
There is a play area and outdoor sports facility within close proximity of the development.	Green verges have been used in some areas, with additional planting on the edge of the development.	The boundary treatments are predominantly informal with lawns and low shrubs and planting to define the public and private spaces.

ARCHITECTURE

Building Heights	Building Types	Roof Materials	Wall Materials	Fenestration
The building heights are in keeping with the surrounding context of 2 storey with occasional 2.5 storey.	Southwest of Newport Pagnell's high street, this area is predominantly made up of detached and semi-detached dwellings with the occasional row of terraced.	Tile roofing is also used throughout	The building materials broadly represent those of the local area, comprising of red brick with occasional render and mock Tudor detailing. The architectural style around this development reflects those of broadly the same era, with many of the dwellings built in the late 1990's.	Canopies and porches have been used to clearly define the front entrances and gables fronting the street occasionally add an asymmetrical interest

NEWPORT PAGNELL QUEEN'S AVENUE

- 3.30 Within close proximity to Newport Pagnell's high street, this area is predominantly terraced and semi-detached dwellings with the occasional detached. The building heights are in keeping with the surrounding context of 2 storey with occasional 2.5 storey. The architectural style is generally within the same post-war period, presenting a compositional rhythm with a horizontal emphasis.
- 3.31 The building materials broadly represent those of the local area, comprising of red brick with occasional render and stone. Tile roofing is also used throughout the area. Chimneys can be found on many of the dwellings with other features such as flat front canopies to clearly define the front entrances. The boundary treatments are predominantly informal with lawns, low shrubs and planting to define the public and private spaces. Car parking is generally placed in front of the dwellings on private drives.



LAYOUT			
Form	Street Corridor	Building Set-back	Parking
This development comprises of a linear form, houses are mainly terraced along the street with occasional back-to-back placed within the block.	There are generous footpaths with green verges and houses are positioned so that they have surveillance of the street.	The dwellings have a consistent set-back from the street with a small lawn and driveway.	Car parking is generally placed in front of the dwellings on private drives for 1 car.

LANDSCAPE		
Public Space Typology	Planting	Boundary Treatment
There is a little public space in this development, which is not surprising, with such close proximity to the high street.	There are few trees planted along the streets, however there is grass at the end of the blocks and green verges.	The boundary treatments are predominantly informal with lawns and low shrubs and planting to define the public and private spaces

ARCHITECTURE				
Building Heights	Building Types	Roof Materials	Wall Materials	Fenestration
The building heights are in keeping with the surrounding context of 2 storey with occasional 2.5 storey.	This area is predominantly made up of terraced and semi-detached dwellings with the occasional detached.	Tile roofing is also used throughout the area.	The building materials broadly represent those of the local area, comprising of red brick with occasional render and stone	The architectural style is consistent broadly within the same post-war period, presenting a compositional rhythm with a horizontal emphasis. Chimneys can be found on many of the dwelling with other features such as flat front canopies to clearly define the front entrances.

MONKSTON

- 3.32 Monkston Park is a relatively new area in Milton Keynes with an abundance of tree lined streets, newly built houses and a large circular park with many houses overlooking this open space. It has its own community hub consisting of a Co-op store, a pre-school, a primary school, and a community centre. Milton Keynes Central is only a 10min drive from the development and directly accessible by public transport, making this an ideal location for commuters.
- 3.33 Streets are predominantly tree lined with generous footways and an abundance of public open space at street intervals. The street pattern follows the circular park, with loops and cul-de-sacs, however, a more linear structure can be seen to the West of the development. The building types are predominantly semi-detached and detached dwellings with occasional terraced. The dwellings are 2 storeys with occasional 2.5 storey and an irregular building separation. Generally speaking there is a generous set back from the street with front gardens and parking at both the front and side of dwellings. These boundaries are predominantly defined by informal lawns and planting, with occasional more formal hedges and walls. Private drives are common in this development with more informal on street visitor parking.
- 3.34 The architectural style for these dwellings is from the same era, with most of the houses being built in the last 20 years. Therefore, the materials consist of red or buff brick with occasional render, decorative brick banding and roof tiles. The dwellings feature predominant chimneys, bay windows, commonly balanced symmetrical and asymmetrical elevations with flat front canopies.



LAYOUT

Form	Street Corridor	Building Set-back	Parking
The street pattern follows the circular park, with loops and cul-de-sacs, however, a more linear structure can be seen to the West of the development.	Since this is a recent development, streets are predominantly tree lined with generous footways.	There is a generally consistent significant set back from the street with front gardens.	There is parking at both the front and side of dwellings. Private drives are also common in this development with occasional more informal on street visitor parking

LANDSCAPE

Public Space Typology	Planting	Boundary Treatment
There is an abundance of public open space at street intervals and a large circular park signifying the centre of the development.	This is a significantly green scheme, there is planting around the development edges, tree lined streets and green verges.	These boundaries are predominantly defined by informal lawns and planting, with occasional more formal hedges and walls

ARCHITECTURE

Building Heights	Building Types	Roof Materials	Wall Materials	Fenestration
The dwellings are 2 storeys with occasional 2.5 storey and an irregular building separation.	The building types consist of predominant semi-detached and detached dwellings with occasional terraced.	A mixture of clay and grey tiles are used.	The architectural style for these dwellings is from the same era, with most of the houses being built in the last 20 years. Therefore, the materials consist of red or buff brick with occasional render, decorative brick banding	The dwellings feature predominant chimneys, bay windows, commonly balanced symmetrical and asymmetrical elevations with flat front canopies.

TICKFORD END SEVERN DRIVE

- 3.35 Severn drive is a primary road with several secondary streets breaking off, this assessment will cover both the primary and secondary streets. The building types found in this character area comprise of predominantly semi-detached with occasional detached housing. The dwellings are also predominantly 2 storeys with occasional 2.5 storey larger houses. Due to many of these dwellings being semi-detached, a regular building separation has been applied throughout with a strong building line.
- 3.36 The architecture is predominantly typical of a post war style with a horizontal compositional rhythm achieved by the dark wooden or white plastic cladding commonly found in this development. The material palette comprises of red and buff brick with roof tiles.
- 3.37 The boundary treatments are generally informal with low planted shrubs and lawn frontages and occasionally more formal fences and walls. Most of the houses benefit from a private drive at the front or side of the dwelling, there is also occasional informal on street parking. Building details include predominant chimneys, occasional bay windows, commonly balanced symmetrical elevations, well defined entrances typically with flat front canopies and commonly used porches.



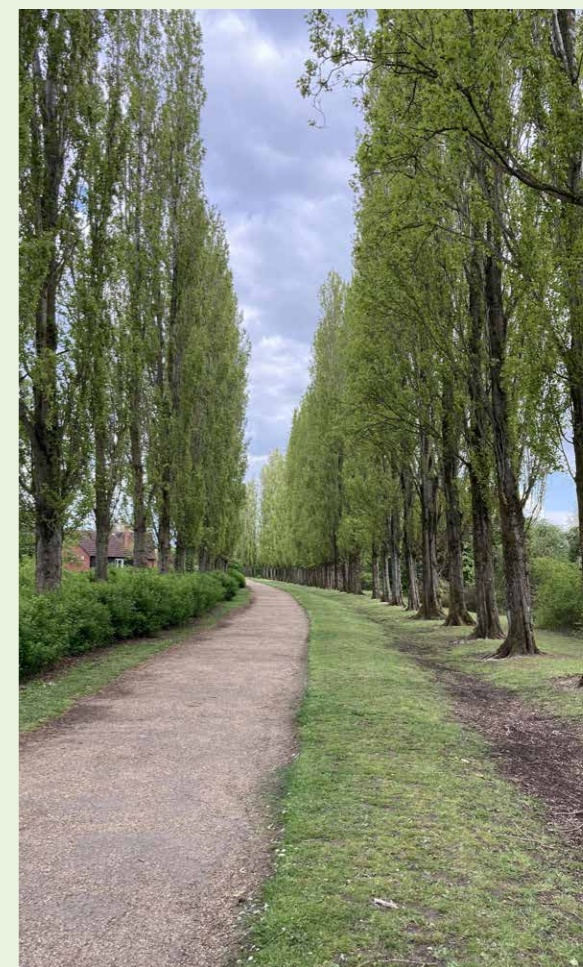
LAYOUT			
Form	Street Corridor	Building Set-back	Parking
Severn drive is a loop road with several secondary streets breaking off, these dwellings are predominantly placed back-to-back.	The street has narrow footpaths, and only one side of Severn Drive is overlooked by active frontage.	Due to many of these dwellings being semi-detached, a regular building separation has been applied throughout with a strong building line.	Most of the houses benefit from a private drive at the front or side of the dwelling, there is also occasional informal on street parking.

LANDSCAPE		
Public Space Typology	Planting	Boundary Treatment
There is little public space within the residential area, however, Tickford Park is a short walk away.	There is a grass verge on the main street and occasional planting on street corners.	The boundary treatments are generally informal with low planted shrubs and lawn frontages and occasionally more formal fences and walls.

ARCHITECTURE				
Building Heights	Building Types	Roof Materials	Wall Materials	Fenestration
The dwellings are also predominantly 2 storeys with occasional 2.5 storey larger houses.	The building types found in this character area comprise of predominantly semi-detached with occasional detached housing	Roof tiles have been used.	The architecture is predominantly typical of a post war style with a horizontal compositional rhythm achieved by the dark wooden or white plastic cladding commonly found in this development. The material palette comprises of red and buff brick	Building details include predominant chimneys, occasional bay windows, commonly balanced symmetrical elevations, well defined entrances typically with flat front canopies and commonly used porches.

HOBART CRESCENT

3.38 A small cluster of dwellings fronting the River Ouzel. The dwellings follow the surrounding architecture, comprising of red brick with dark wood panelling and rustic wooden balconies with a view of the water. They are all 2 storeys in height with roof tiles and occasional chimneys. Parking is largely in view, at the front and sides of the dwellings, with all houses having their own rear garden and small front lawn. The boundary treatments are informal with low shrubs and a line of tall trees to separate the cluster of dwellings from the public footpath along the River Ouzel. There are several small clusters of dwellings fronting the River Ousel, including 3 storey apartments at Gyosei Gardens.



LAYOUT			
Form	Street Corridor	Building Set-back	Parking
A small cluster of dwellings fronting the River Ouzel.	The streets leading to the dwellings are well-overlooked and have a good view of the tall trees which frame the River-fronting houses.	All houses having their own rear garden and small front lawn.	Parking is largely in view, at the front and sides of the dwellings

LANDSCAPE		
Public Space Typology	Planting	Boundary Treatment
These dwellings front onto the River with a pedestrian trail running alongside.	This secluded area is very close to nature, benefiting from green and blue infrastructure. There is an abundance of trees and planting surrounding these dwellings.	The boundary treatments are informal with low shrubs and a line of tall trees to separate the cluster of dwellings from the public footpath along the River Ouzel.

ARCHITECTURE				
Building Heights	Building Types	Roof Materials	Wall Materials	Fenestration
They are all 2 storeys in height with roof tiles and occasional chimneys	Semi-detached houses.	Clay roof tiles	The dwellings follow the surrounding architecture, comprising of red brick with dark wood panelling and rustic wooden balconies with a view of the water.	The ground floor comprises of small vertical windows, whilst the first floor has a balcony door and an additional vertical window. The window placement is asymmetrical and there is a mixture of brown and white windows used.



DESIGN INFLUENCES TO BE TAKEN FORWARD

LAYOUT

Form	Street Corridor	Building Set-back	Parking
Development overlooking existing vegetation and landscape so that the development creates connections with the existing context	<p>Tree lined streets with generous footpaths overlooking well maintained public realm</p> <p>Development fronting the primary route, ensuring streets are well overlooked which in turn provides safety to pedestrians and residents</p>	Should vary with the character to be created. More generous to green edges and areas of development overlooking open space	At the front or side of dwellings to avoid cars being obtrusive to the street scene. On streets away from main routes opportunities for frontage parking should be utilised to increase accessibility to space

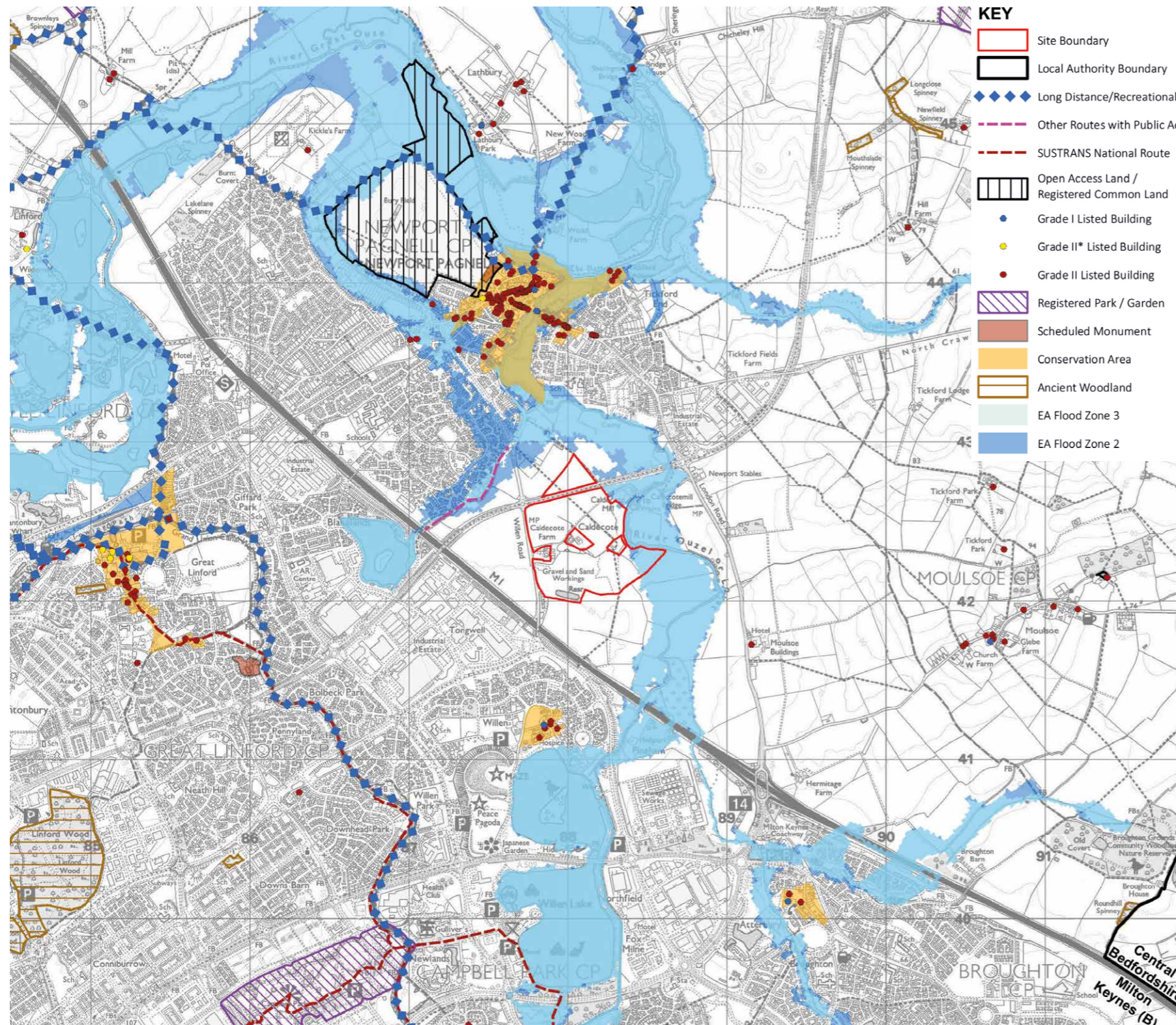
LANDSCAPE

Public Space Typology	Planting	Boundary Treatment
Feature landscaped spaces such as parks in the centre or at the edge of residential areas	This secluded area is very close to nature, benefiting from green and blue infrastructure. There is an abundance of trees and planting surrounding these dwellings.	The boundary treatments are informal with low shrubs and a line of tall trees to separate the cluster of dwellings from the public footpath along the River Ouzel.

ARCHITECTURE

Building Heights	Building Types	Materials	Detailing
<p>Clusters of dwellings of the same material or height to create a memorable character</p> <p>Predominantly 2-2.5 storey dwellings, in keeping with the surrounding context</p>	<p>A predominantly detached and semi-detached development with the occasional terraced units</p> <p>Principal entrances to dwellings are instantly recognisable and animated. Generally speaking development should front the higher category street typology</p>	Red brick dwellings with roof tiles are common with the occasional use of render, to reflect the vernacular architectural style in the surrounding context	Projecting front gables and a mixture of symmetrical and asymmetrical elevations have been seen throughout the developments to provide interest to street scenes

SITE STUDIES



ENVIRONMENTAL DESIGNATIONS PLAN

LANDSCAPE AND VISUAL IMPACT

“Well-designed new development is integrated into its wider surroundings, physically, socially and visually. It is carefully sited and designed, and is demonstrably based on an understanding of the existing situation...”

(Para. 43, NDG 2021)

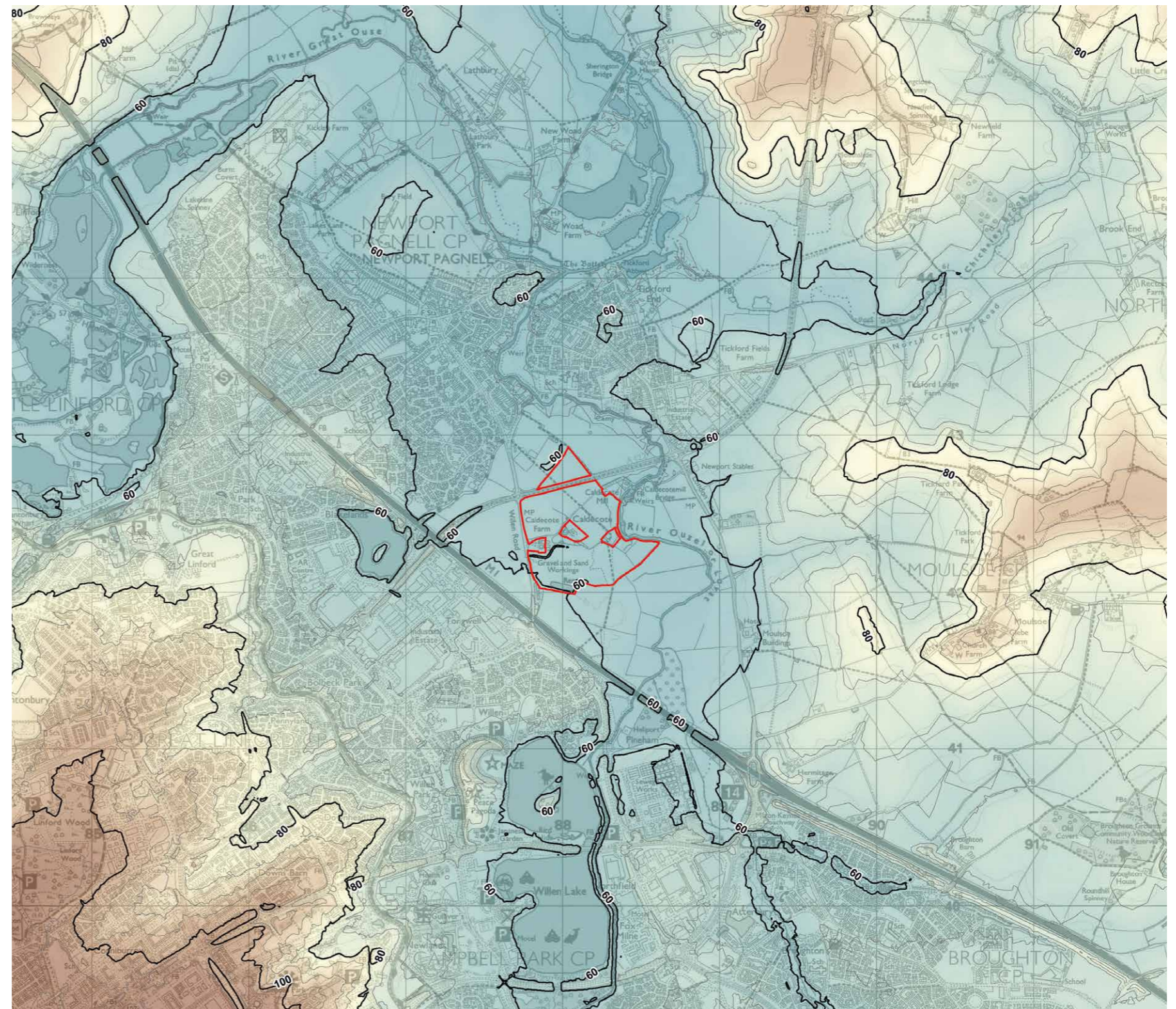
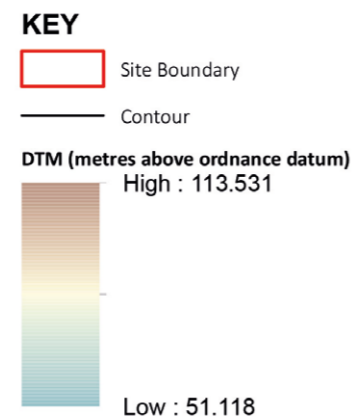
- 4.1 Pegasus Group have undertaken numerous site visits and desk-based assessments to identify the key viewpoints and constraints affecting the development (outlined in further detail in the LVIA and ES and associated appendices). The LVIA concludes that there would be some localised significant visual effects due to proximity and direct nature of views, gained from properties and Public Rights of Way within the site during construction and at year 1 operation.
- 4.2 None of the remaining visual receptors within the study area, however, have been assessed as experiencing significant visual effects. In addition, none of the landscape character areas or landscape elements of the Site including the River Ouzel have been assessed as being subject to significant effects, including cumulative landscape effects.
- 4.3 Overall, the LVIA concludes that the proposed development responds well to the characteristics of the receiving environment, mitigating visual effects, whilst not compromising the requirements of the proposed development.

HERITAGE AND ARCHAEOLOGY

4.4 The accompanying archaeological work undertaken by Triskelion has concluded that;

“The proposed development would have no adverse indirect effect on or harm the significance of any other non-designated or designated heritage asset. With respect to the cultural heritage of the built environment the Planning (Conservation Areas and Listed Buildings) Act 1990 does not apply as no harm has been identified to the significance of a Listed Building arising from development within its setting. In determining the application, the Council’s duty to have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses is disengaged.”

4.5 Further detail regarding the archaeological baseline work undertaken is set out in the ES.



TOPOGRAPHY PLAN



TREE CONSTRAINTS PLAN

ARBORICULTURE

4.6 An arboricultural survey has been undertaken across the site and the results are summarised in the table below:

Category	Trees	Groups	Hedges
A	3	1	-
B	36	12	-
C	84	50	8
U	14	-	-

4.7 The proposals should seek to minimise tree loss across the development wherever ever possible.

4.8 The proposals should include additional proposed tree planting to mitigate against trees lost to facilitate development.

ECOLOGY AND BIODIVERSITY

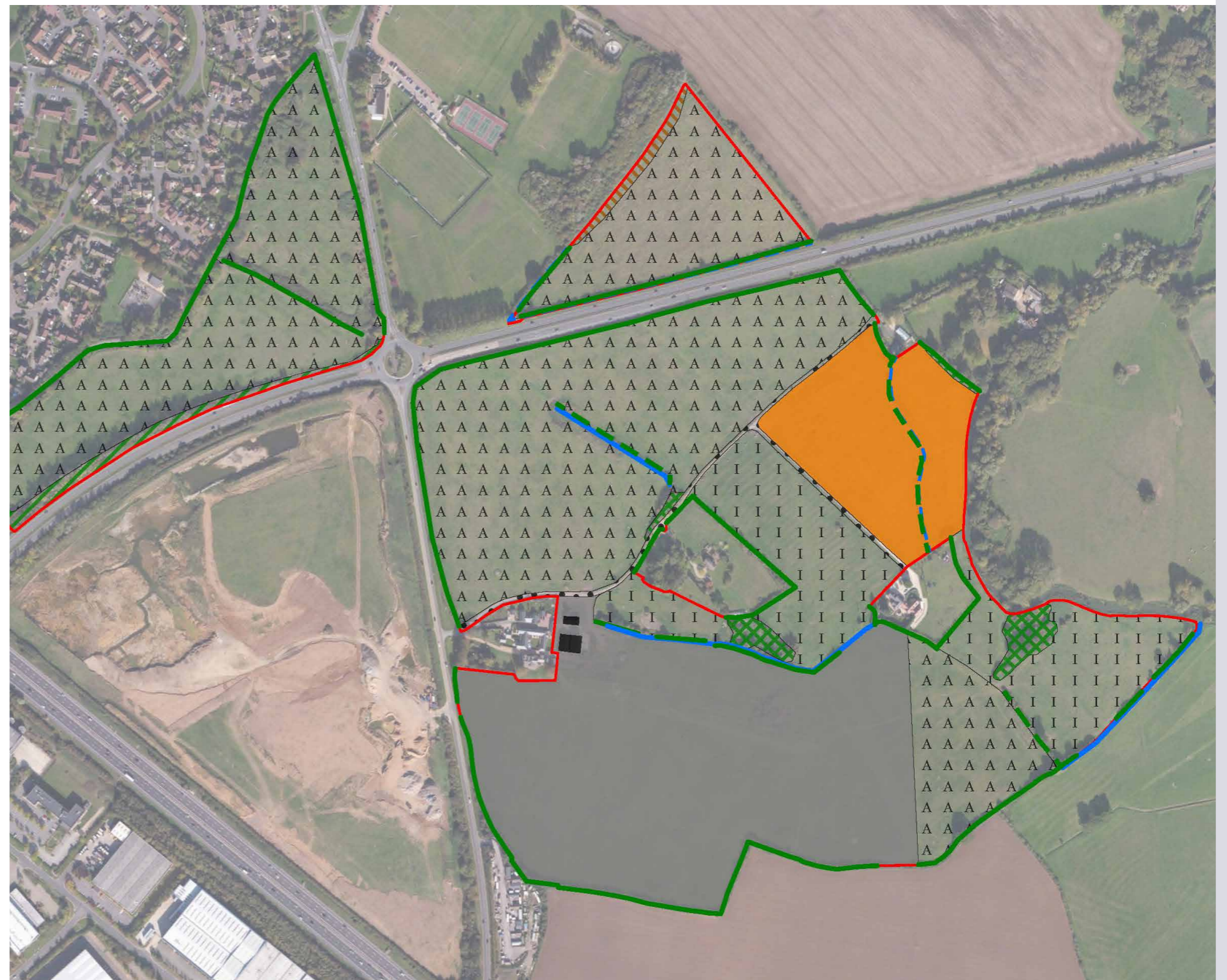
- 4.9 RSK undertook a number of ecological surveys of the site, which are detailed further in the ES. Following these they have concluded that;

“The development of the Site will take place on habitats that primarily include arable and improved grassland. An area of species-rich lowland meadow will be lost but a bespoke habitat creation scheme will see new lowland meadow created. The River Ouzel corridor will be maintained in its entirety together with a suitable buffer and this together with the creation of grassland and attenuation lagoons will provide continued foraging habitat for bats and farmland birds. As a precautionary measure it has been assumed great crested newts are present and use will be made of the local District Licensing Scheme to offset any potential impacts on great crested newt.”

- 4.10 Subject to suitable mitigation outlined within the ecological reports and in the ES the proposed development will not cause unacceptable harm to biodiversity species or habitats.

Legend:

- Site Boundary
- Broad-leaved Plantation Woodland
- Scrub
- Lowland Meadow
- Improved Grassland
- Tall Ruderal Vegetation
- Arable
- Building
- Hardstanding
- Bare ground
- Hedge
- Gappy Hedge
- Wet Ditch
- Dry Ditch



PHASE 1 HABITAT PLAN

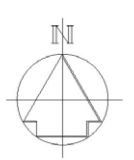
AIR QUALITY

- 4.11 An assessment of the potential air quality and dust impacts of the construction phase of the proposed development has been undertaken.
- 4.12 During the construction phase, impacts of the proposed development may potentially arise due to fugitive dust and particulate matter emissions. The risk was assessed according to a widely used method published by the Institute of Air Quality Management (IAQM), 'Guidance on the assessment of dust from demolition and construction'.
- 4.13 Mitigation measures have been recommended to reduce the dust risk for general site activities and construction-specific activities, as well as emissions from plant associated with construction related activities. With the implementation of the appropriate measures, no significant impacts are anticipated.

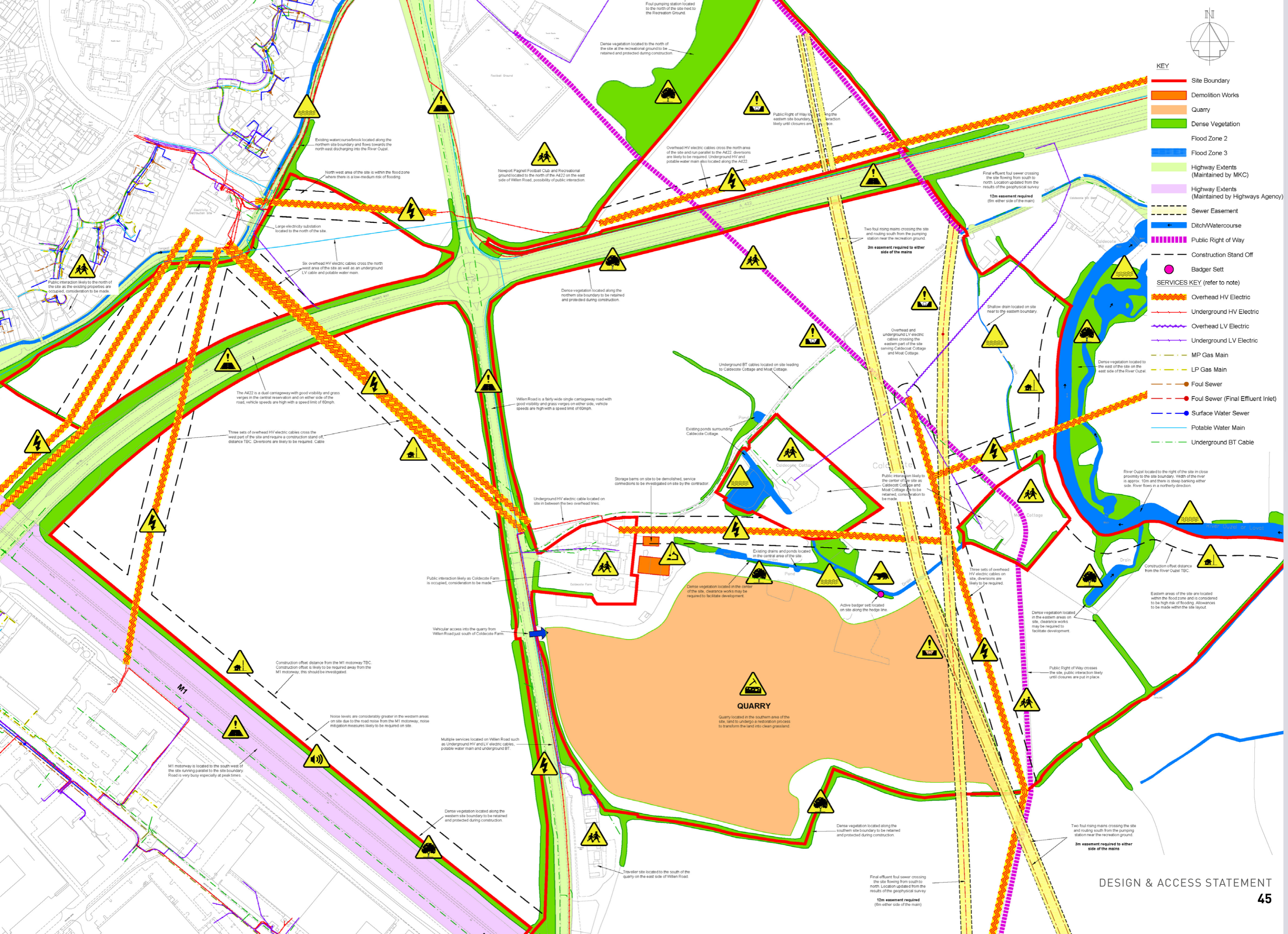
EXISTING UTILITIES

- 4.14 An assessment of the existing utilities crossing the site has been undertaken. The location of these services, associated constraints to development and if there is any capacity in the local network to accommodate the future loads from the proposed development are set out in the table below and the plan opposite:

Service	Utility present or in vicinity of the site	Does the utility constrain the development?	Can the development be served?
Electricity	Yes There are Overhead High Voltage (11 kV & 33 kV) lines running across the site. There are also underground and overhead HV & LV cables crossing the development	Yes Diversionary works are required to facilitate the off site highway improvements and also the development of the site	Yes Connections to be made to the existing HV network crossing the site. Three substations are required to serve the development
Gas	No	No	Yes Subject to off-site main being laid to the connect to a medium pressure main located at the junction of Wolverton Road with Marsh End Road. The off-site main is some 1460m from the development.
Water	Yes Mains running in Willen Road and along the A422	Yes Diversionary works are required to facilitate the off site highway improvements.	Yes Subject to off-site improvements, which are funded through the infrastructure charges levied by AWS.
Foul Water Drainage	Yes Existing trunk mains cross the development	Yes There is sufficient capacity at the water recycling centre to serve the development.	Yes A foul water pumping station is required to connect to the designated point of connection.
Telecoms	Yes There are multiple networks running in the existing public highway	Yes Diversionary works are required to facilitate the off site highway improvements.	Yes High speed broadband is available once planning permission has been obtained an appropriate provider will be appointed.



- KEY**
- Site Boundary
 - Demolition Works
 - Quarry
 - Dense Vegetation
 - Flood Zone 2
 - Flood Zone 3
 - Highway Extents (Maintained by MKC)
 - Highway Extents (Maintained by Highways Agency)
 - Sewer Easement
 - Ditch/Watercourse
 - Public Right of Way
 - Construction Stand Off
 - Badger Sett
- SERVICES KEY (refer to note)**
- Overhead HV Electric
 - Underground HV Electric
 - Overhead LV Electric
 - Underground LV Electric
 - MP Gas Main
 - LP Gas Main
 - Foul Sewer
 - Foul Sewer (Final Effluent Inlet)
 - Surface Water Sewer
 - Potable Water Main
 - Underground BT Cable



HYDROLOGY AND DRAINAGE

EXISTING TOPOGRAPHY AND DRAINAGE

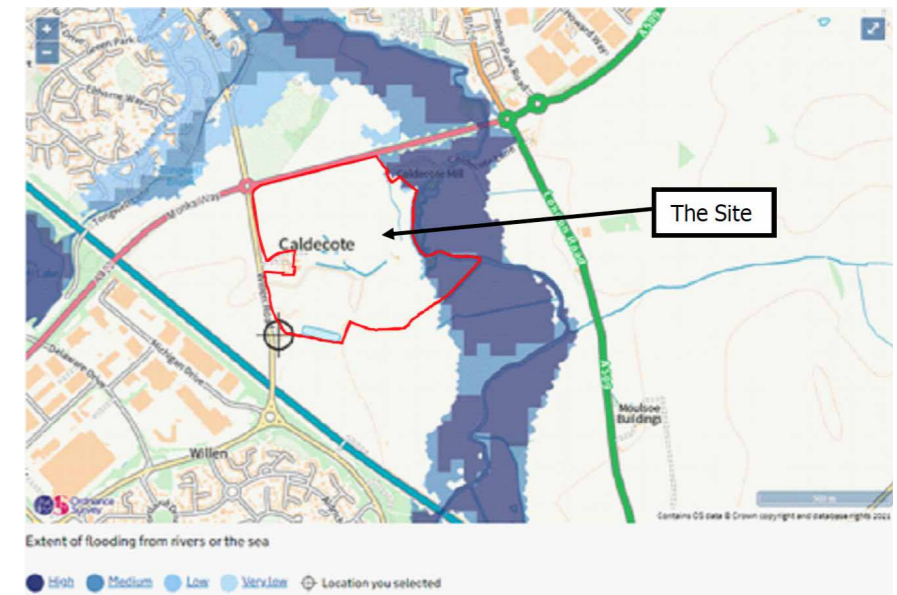
- 4.15 The site has a gradual fall from west to east with levels ranging from approximately 60.00m AOD down to 55.89m.

EXISTING FLOOD RISK

- 4.16 The flood mapping available from the Government website (below) shows that The majority of the site is located within Environment Agency (EA) Flood Zone 1, as such there is no risk of fluvial flooding to the majority of the site. T
- 4.17 A small part of the north east corner is located within EA Flood Zone 2 and has a medium risk of flooding. The easternmost part of the site is located within EA Flood Zones 2 and 3 and has a high risk of fluvial flooding.
- 4.18 There is no low to high risk of flooding from surface water within the site.
- 4.19 There is a high risk of reservoir breach flooding within two thirds of the site from Willen Lake and Tongwell Lake which are approximately 1.65km south of the site. The Reservoirs Act 1975 ensures that reservoirs are regularly managed, maintained and inspected which makes reservoir breaches rare. Although some of the site is within the maximum extent of reservoir flooding, the risk of reservoir flooding is considered to be low.

SUSTAINABLE SURFACE WATER DRAINAGE

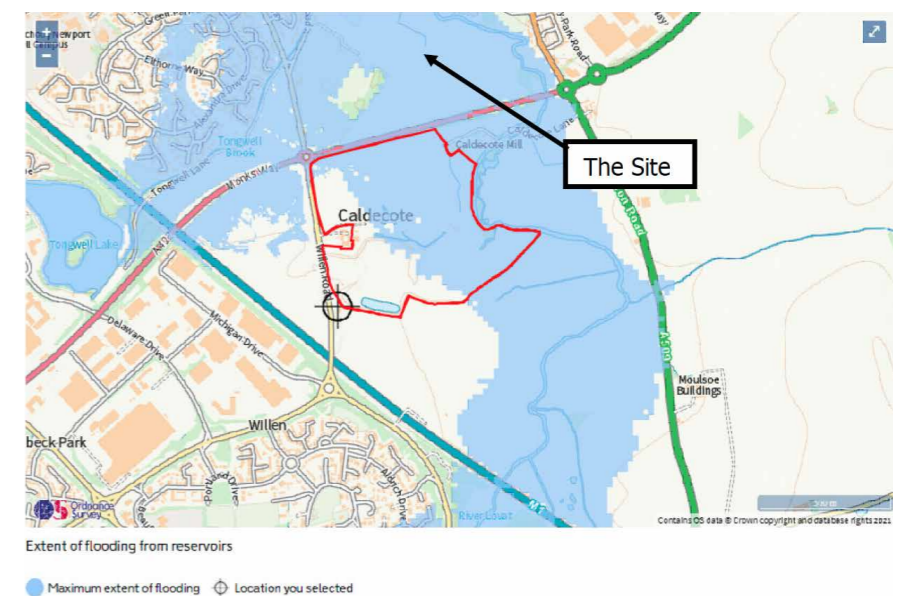
- 4.20 The surface water drainage systems are proposed to be designed in accordance with nationally agreed standards, and will provide protection from surface flooding under the critical 100 year rainfall event, including the recognised allowance for the effects of climate change.
- 4.21 The main drainage systems are proposed to be offered for adoption and to be maintained by Anglian Water. This will ensure long term maintenance throughout the life of the development.
- 4.22 As with any new development, and in accordance with requirements of the Building Regulations and other national guidance, the first method which should be considered for the disposal of surface water is by infiltration into the ground.
- 4.23 A total of 14 soakaway tests were undertaken in September 2020 at different locations across the site. Although infiltration rates vary across the site, the infiltration rates are generally poor. It is therefore likely that soakaway drainage will not be suitable for the disposal of surface water.
- 4.24 It is proposed that the surface water for the site will be restricted to greenfield run-off rates and will discharge into the River Ousel to the east of the site.
- 4.25 The site will be split a series of catchments, and each will discharge to a proposed attenuation pond which will be sized to cater for the 100 year + 40% Climate Change Storm Event. Flows from each pond will then be restricted using a flow control device. For catchments in the east of the site the restricted flows will go directly to the existing River Ousel. For those in the west of the site the restricted flows will pass through a proposed swale which then outfalls to the existing River Ousel.
- 4.26 The surface water drainage outfall to the River Ousel will be subject to EA approval.



EXISTING EXTENT OF SURFACE WATER FLOODING



EXISTING EXTENT OF SURFACE WATER FLOODING



EXISTING EXTENT OF FLOODING FROM RESERVOIRS

NOISE

- 4.27 An AVO Level 1 assessment has been conducted showing that dwellings located close to the A422 are 'high' with regard to noise during periods of overheating.
- 4.28 An initial AVO Level 2 assessment has been undertaken which has shown that with suitable acoustic vents to all habitable rooms within the 'high' risk area internal noise levels during periods of overheating can be suitably controlled.
- 4.29 During cooler periods it has been found that a suitable level of amenity can be provided within the proposed residences, with acoustically enhanced glazing and ventilation openings necessary for some dwellings. Acoustic performance requirements for the relevant façade elements are provided in the ES chapter and appendices.

EXTERNAL AMENITY AREAS

- 4.30 It is recommended that all garden spaces are located so that the houses provide acoustic screening between the garden and the nearest existing roads. Where this is done all gardens would be expected to have an area which achieves the aspirational criterion of <55 dB LAeq 16h.
- 4.31 Where the layout means that some gardens have a view of the A422 it will be necessary to provide acoustic screening from the road with a 2m high acoustic fence. The detailed design of dwellings and the acoustic fence will be undertaken at the RMA stage.

GROUND CONDITIONS

BASELINE CONDITIONS

- 4.32 The site has a history of use predominantly as farmland and no significant contamination has been identified in the ground across the majority of the site.
- 4.33 The former quarry has been backfilled with inert materials and testing shows the fill material to be largely free of significant contaminants. Some low hazardous ground gas presence has been detected in the former quarry but this does not appear extensive and the rate of gas generation appears to be very low.
- 4.34 A single local area of slightly contaminated materials has been identified on the edge of the former quarry which probably relates to buried farm wastes rather than recently placed backfill to the former quarry.
- 4.35 There are no industrial activities at the site or in the immediate vicinity likely to cause serious release of contaminants to the ground or groundwater.
- 4.36 Backfilled former sand and gravel working in the vicinity are considered to have a very low potential for generation of hazardous gases or for mobile chemical contaminants to impact the Application Site.

MITIGATION AND ENHANCEMENT

- 4.37 The potential effects of the development will be mitigated by the following measures:

PRE-CONSTRUCTION

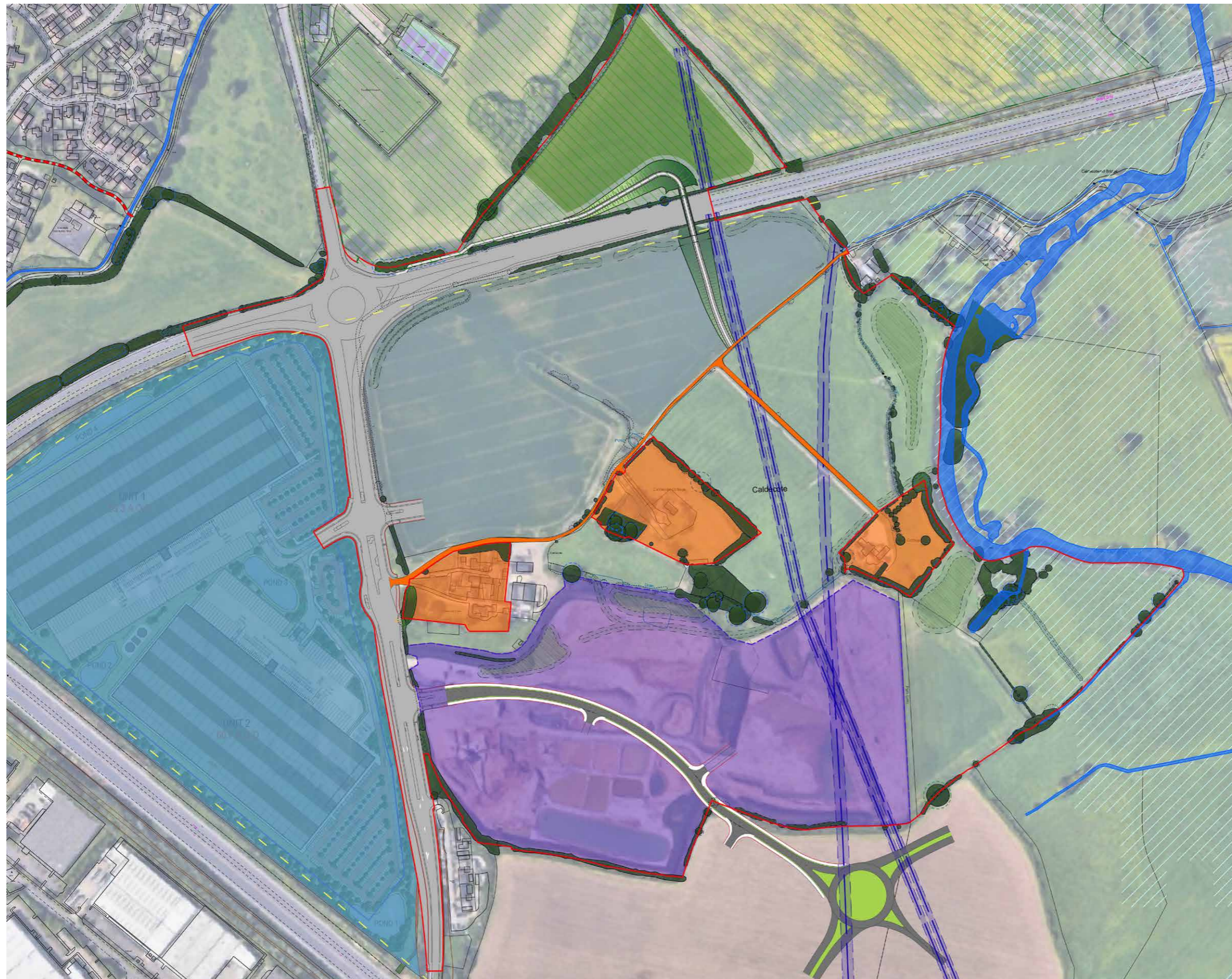
- 4.38 Gas monitoring is to continue until at least 6 rounds have been undertaken and a reasonably clear gas regime has been characterised.
- 4.39 An updated full Geotechnical and Geo-environmental Report will be produced on completion of the gas monitoring – this will confirm remediation requirements and/or mitigation measures and provide the basis for a Remediation Strategy with respect to contamination presence.

CONSTRUCTION

- 4.40 A Construction Management Plan will be prepared which will specify how works are to be safely undertaken on site including how to undertake any remediation or mitigation works to control possible risks from contamination. Typically, any special equipment or plant will be specified; working methods to prevent contamination release will be indicated; any monitoring or other controls will also be specified.
- 4.41 Any Remediation Strategy will normally be subject to a specific planning condition and its successful completion will need to be recorded and demonstrated in a Verification Plan – again subject to planning approval.
- 4.42 A Waste Management Plan will be required to specify how wastes are to be reduced and managed including handling, collection, storage, recycling and disposal.

OPERATION

- 4.43 The main mitigation measure to ensure effects are negligible will be the completion of the Remediation Strategy and then the Verification Report.
- 4.44 It is considered extremely unlikely that any on-going monitoring of contaminants will be required during occupation or any form of maintenance of remediation/ mitigation measures required.
- 4.45 The assessment concludes that following suitable further site investigation works and the implementation of likely simple mitigation measures the site will be suitable for development with no significant adverse effects to human health, groundwater, surface water or the ecological environment.



- KEY**
- Site Boundary
 - MK East SUE boundary
 - Flood Zone 3 within the site
 - Proposed MK Grid road
(Fig 4.2 MK East SUE Development Framework SPD)
 - Existing PRoW (subject to potential diversions)
 - Existing MK redway
 - Existing pedestrian/cycle route
(leisure/traffic free/quiet route)
 - Adjacent employment development
(Newlands)
 - Existing rising mains + associated easements
 - Existing sewer + associated easement
 - Existing vegetation with associated RPZ
 - Indicative attenuation features
 - Overbridge to provide safer crossing point across
A422 for existing PROW. A small element of diversion
is required.
 - River Ouzel or Lovat
 - Existing properties within allocation area but
excluded from site proposals
 - Strategic Highways improvements and access
provided by RPS
 - Access track serving existing properties
 - Potential noise impact
 - Extent of quarry.
Existing quarry operations present a
constraint on the timing of development-
operations need to cease and land needs time
period to settle post filling

OPPORTUNITIES AND CONSTRAINTS PLAN

OVERVIEW OF THE SITE AND CONTEXT

4.46 The results of the various site studies are used to inform and structure the development proposals. These are outlined below and illustrated, where appropriate, on the Opportunities and Constraints plan presented opposite.

LAND USE

- Opportunity to provide Public Open Space (POS) integrated alongside residential development
- Opportunity to provide a significant new riverside linear park, connecting with the public open space provided as part of the wider SUE allocation
- Opportunity to provide a new local centre
- The proposals will respect the amenity and privacy of existing properties located adjacent to the site boundary
- Opportunity to deliver playing pitches to the north of the A422, in accordance with NPNP Policy NP8
- Opportunity to deliver a significant area of the strategic residential-led/mixed-use SD12 Milton Keynes East allocation, meeting the housing need of Newport Pagnell and Milton Keynes, including the delivery of affordable housing; and

ACCESS

- Opportunity to provide a vehicular access via a new access points in the east of the site, from Willen Road
- Opportunity to provide links from the application site to the wider SD12 Allocation site, enabling the integrated delivery of future development
- Opportunity to provide a new traffic free safe overbridge north over the A422, linking the site to Newport Pagnell and Willen Road Sports Ground
- Opportunity to promote the use of sustainable methods of transport, and the development should ensure that access and links via pedestrian, cycle and public transport are attractive and direct; and
- Opportunities to enhance the existing Public Rights of Way (PRoW) network with appropriate diversions where necessary to improve connectivity

UTILITIES

- The existing sewers, and associated easements, will be retained in-situ within the proposals; and
- Opportunity to provide new foul pumping stations to support the developments needs.

DRAINAGE

- The majority of the site falls within Environment Agency (EA) Flood Zone 1. Flood Zones 3 encroaches on the east of the site. Development will not be located within Flood Zone 3
- Opportunity to utilise Sustainable Drainage systems (SuDs) to attenuate and better manage surface water run-off.

ARBORICULTURE

- Existing tree and hedgerow planting will be retained and enhanced within the proposals wherever possible; and
- Opportunities to provide additional tree planting around the eastern site boundary and the central corridor.

NOISE

- The proposed development will be set back from the northern and western site boundaries with the A422 and Willen Road, to mitigate against any potential noise impacts
- Opportunity to provide noise fence, screened with planting, along the western and northern site boundary.



INDICATIVE CONCEPT PLAN

- Grid Road
- Existing Road to be Retained and Upgraded to Grid Road
- Local Distributor Road
- Fast Mass Transit
- Primary Residential Street
- London Road to be Downgraded
- Redway
- Park and Ride
- Housing
- Employment
- Mixed Use Community Hub
- Local Centre
- Linear Park
- Open Space Link
- Green Buffer
- Indicative Location of Playing Fields
- Potential Off-site Location of Playing Fields
- Indicative Location of Secondary School
- Indicative Location of Primary School
- Proposed Road Bridge
- Pedestrian/Cycle Connection
- New Junction (Detailed Location and Form to be Determined)
- SUE Boundary
- Pedestrian Priority Street

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DEVELOPING THE DESIGN CONCEPT

“Design quality should be considered throughout the evolution and assessment of individual proposals. Early discussion between applicants, the local planning authority and local community about the design and style of emerging schemes is important for clarifying expectations and reconciling local and commercial interests. Applicants should work closely with those affected by their proposals to evolve designs that take account of the views of the community. Applications that can demonstrate early, proactive and effective engagement with the community should be looked on more favourably than those that cannot.”

(Para. 132, NPPF 2021)

SUSTAINABLE STRUCTURING

- 5.1 In line with National and Local Government Guidance and Policy, considerable importance has been placed on achieving a high standard of design across the site. Successful urban design is dependent upon achieving an appropriate relationship between community needs, development principles, development form and a positive response to local conditions.
- 5.2 Plan-makers, as well as decision makers should apply a presumption in favour of sustainable development, which will mean that:

“All plans should promote a sustainable pattern of development that seeks to: meet the development needs of their area; align growth and infrastructure; improve the environment; mitigate climate change (including by making effective use of land in urban areas) and adapt to its effects”.

(Para 11(a), NPPF 2021)

- 5.3 The application of key urban design objectives will ensure a high-quality layout is achieved, whilst the early identification of the sites features will ensure that the proposals are sensitively assimilated into the landscape and urban fabric.
- 5.4 To ensure a site-specific response that achieves the aspirations of the SUE framework and the policy requirements of SD12 the following placemaking layers have been incorporated from the outset.



SUE CONCEPT PLAN

- 5.5 The SUE Concept Plan (taken directly from the Development Framework SPD) and accompanying text sets out the design aspirations for the SUE. These being:
- A linear park based around the River Ouzel corridor;
 - A landscape buffer to Moulsoe;
 - A mixed use community hub at the heart of the main residential area;
 - A secondary school close to the community hub;
 - Four primary schools spread equidistantly around the residential areas;
 - A new road bridge over the M1 providing an improved link to CMK and the urban area of MK; and reducing pressure on the A422;
 - Safeguarded route for a fast mass transit route;
 - A park and ride site;
 - Employment development along the edge of the motorway;
 - Pedestrian/cycle connections across the M1 and A422;
 - An outer road to allow through traffic to move through the site without conflicting with areas of housing and the people-centric places within the site;
 - Willen Road to be retained and upgraded to a grid road;
 - Downgrade of A509 London Road through the site to avoid it becoming a through route.
- 5.6 It should be noted that para 1.4.4 of the Development Framework SPD enables deviations from the set out strategy by stating:

“The Development Framework provides guidance and further detail to the development principles set out in the adopted Plan:MK. Alternative solutions and land use arrangements could come forward as part of the planning application process and should explain the reasons for any significant differences in approach.”



EXISTING VEGETATION

Existing vegetation is generally contained to the sites boundaries. Detailed surveys are being undertaken, the findings of which will be used to further refine the development proposals and will accompany the application submission.



EAST – WEST GREEN CORRIDOR

Running broadly to the north of the existing quarry, this strategic area of open space provides a vehicular free, albeit there will be occasional road crossing points, route from the western site boundary to the eastern.



NORTH – SOUTH GREEN CORRIDOR

Running along the route of existing underground utilities and their associated wayleaves, this greenway will provide a pedestrian route from the northern boundary to the southern. The intention being that a pedestrian crossing point to the potential sports pitch location is to be explored.



RIVER OUZEL PARK

This area of strategic open space is located along the eastern boundary of the site and correlates with the route of the River Ouzel and its associated Flood Zone. The intention is to also locate some of the attenuation features required by the proposed development within this area to further enhance the Riverside character of the parklands.



STRATEGIC LANDSCAPE

Areas requiring strategic landscaping have been identified. These being generally adjacent to the existing highways network and around the existing properties to be retained. These landscaping areas will enable screening opportunities as well as providing a verdant landscaped backdrop to the proposals.



SPORT PITCHES

Sports provision is intended to be located within a self contained northern parcel of land to the north of Monks Way. This will serve both the new and the existing residents in the locality. The intention is to explore the potential for a pedestrian link from the main development area to the pitches via the north-south green link.

- 5.7 The following spreads set out how the masterplan proposals achieve the policy requirements established by SD12 and the SUE Concept plan.

BUILDING THE LAYERS 1: LANDSCAPE

POLICY SD12 REQUIREMENT C7:

“A strategic green infrastructure framework and network of green spaces to meet strategic and local infrastructure requirements that follows guidance in the Council’s Landscape Character Assessment and Green Infrastructure Strategy to ensure ecological connectivity; protect the identity and character of nearby settlements and mitigate any significant impacts on the landscape in accordance with Policy NE5.”

POLICY SD12 REQUIREMENT C8:

“The creation of a linear park through the site that broadly correlates with the River Ouzel floodplain and existing green infrastructure assets of value within and adjacent to it.”

- 5.8 In order to achieve the above stated requirements of Policy SD12 within the application sites capacity, a series of landscape characteristics have been established to create a comprehensive landscape strategy that will guide the placement of development, and ensure a truly landscaped approach to masterplanning.



VEHICULAR
MOVEMENT
STRATEGY

BUILDING THE LAYERS 2: MOVEMENT

POLICY SD12 REQUIREMENT C4

“The phased introduction of a comprehensive network of transport infrastructure in line with the Local Investment Plan, to include grid road connections to H4/V11 to the west and improved highway connections to Newport Pagnell and Central Milton Keynes (CMK), including new and/or enhanced vehicular crossings of the M1, involving highway works on and off-site.”

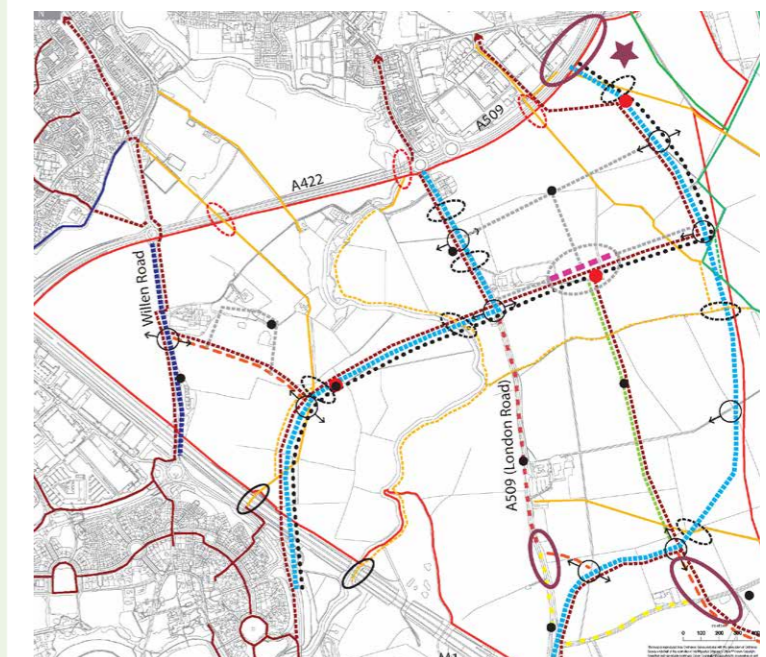
POLICY SD12 REQUIREMENT C5

“A corridor of land safeguarded for a fast mass-transit system, and associated infrastructure, enabling connectivity to CMK and other key destinations. The width of the corridor should be sufficient to enable a range of possible transit solutions to come forward whilst also ensuring the efficient use of land for achieving the scale of development proposed within this policy.”

- 5.9 With the initial landscape framework to guide the development proposals established the next step is to ensure that the above stated vehicular movement aspirations could be met within the context of the application site. It should be noted that requirement C5 lies to the south of the site boundary, however an appropriate boundary response will be required both for the benefit of the proposed development and those of the neighbouring SUE Parcel.

- 5.10 The Vehicular Movement Strategy shown opposite incorporates the following key design features:
- Highways improvements to the existing road network to help mitigate for the increased road activity as a result of the SUE and in line with the Local Investment Plan;
 - Consideration of the CMK Grid Road which lies to the south of the application area and in particular creating an appropriate edge treatment which does not create development inefficiencies for both the application site or the adjacent SUE parcel;
 - 2no proposed access junctions along Willen Road which have been considered alongside the requirement to access the SUE Employment site and the existing access points;
 - Initial highways design for the principal distributor road from the southernmost access point which in the fullness of time allow for connection to the CMK grid road;
 - Indicative route for the principal route through the development proposals and how it will form a loop between the two proposed access points. Its placement has carefully considered the development parcel depths, connectivity to the wider development area as well as individual development parcels and initial place making tools such as verges/carriageway width; and
 - Consideration at an early stage of the access rights held by the existing properties adjacent to the application site across the existing tracks which serve them.

- 5.11 The SUE Concept Plan indicates a single point of access via a Local Distributor Road from Willen Road to proposed Grid Road. A Primary Residential Street then provides a loop from the distributor road. The proposals provide the direct link from Willen Road to the Grid Road, however as an alternative the masterplan proposes the Residential Street to provide a secondary point of access from Willen Road. The route then provides access to the residential areas and connect back to the Willen Road-Grid Road link. This increases both vehicular and pedestrian permeability through the site, giving users options for travel.
- 5.12 Furthermore, there has been considerable liaison with the developers of the employment land to the west of Willen Road, and the wider SUE, to ensure that these arrangements are complementary.



**SUE
CONCEPT
PLAN:
VEHICULAR
ACCESS**



POLICY SD12 REQUIREMENT C6

“A network of segregated, and where appropriate grade-separated, new and enhanced footpaths, cycleways and bridleways (including redways) to connect to existing routes, beyond the site, including provision of appropriate pedestrian and cyclist crossing of the A422 and suitable safe and attractive crossings of the M1 as appropriate.”

- 5.13 In terms of Pedestrian connectivity the following considerations are guiding the on going evolution of the development proposals;
- 2no PROWs enter and traverse the application site, both in a broadly north – south direction and generally contained within the eastern portion of the application site. These two rights of way are to be retained in situ and where appropriate enhanced. The intention being that these routes for their majority are set within open space;
 - Pedestrian crossing points from the application site north towards the sports pitches are being explored; and
 - The elements of strategic open space (i.e. greenways) and the landscaped buffer areas to the site peripheries offer the opportunity to provide vehicular free pedestrian routes around and across the majority of the application site whilst also allowing for the maximisation of off road pedestrian connectivity to the wider SUE. The principal route through the development proposals is also intended to provide an attractive pedestrian route through the proposals that connects all the differing uses.
 - The pedestrian movement strategy going forward is to ensure the opportunities for pedestrian travel are maximised and that a variety of route options are enabled, be it off road or adjacent to the highway.

PEDESTRIAN MOVEMENT STRATEGY

BUILDING THE LAYERS 3: PLACEMAKING

POLICY SD12 REQUIREMENT C1

“Delivery of around 5,000 new homes, including at least 1,475 homes within the plan period, providing a range of sizes, types and tenures, including affordable housing, in accordance with other policies in the Plan.”

POLICY SD12 REQUIREMENT C3

“Associated infrastructure including primary and secondary education, community facilities, health, retail and local services and a hotel. The development should comprise at least one district and/or local centre(s), of a scale commensurate to the needs of the new community and that would not adversely affect the viability and vitality of Newport Pagnell district centre, with co-location of key facilities.

- 5.14 The Development Areas plan shown on the following page demonstrates how once the landscape and movement layers are placed, there leaves distinct areas of development. The majority of which is proposed for residential, meaning the application site has the ability to provide approximately 760 of the required 5,000. This represents about 50% of the 1,475 required during the initial plan period.
- 5.15 The intention is to provide a two form entry primary school (2.2ha site) and a local centre (1ha site) within this application area to serve the northern portion of the SUE and provide relatively early delivery of supporting community facilities.

- 5.16 During the pre-application consultation process discussions were had regarding the potential location of the local centre and primary school, along with an analysis for each option to inform future conversations.
- 5.17 The SUE Concept Plan suggests the location of the Primary School and Local Centre in a more northerly position than proposed. The locations as proposed and the SUE locations considered are set out here:



LOCAL CENTRE/ PRIMARY SCHOOL: MASTERPLAN LOCATION

ANALYSIS:

- Community uses are co-located within a single development parcel, thus journeys to the community hub could be minimised;
- They are located in a visible location adjacent to Willen Road and the proposed primary distributor road as well as the Employment area enabling the local centre to further benefit from passing trade;
- When considered in the context of the wider SUE, the community hub is in a central position to serve the initial wider SUE phases. However, when viewing the application site in isolation they are not central to application proposals; and
- Careful consideration over how pedestrians from the north will access the facilities will need to be taken and the potential for crossing points across the distributor road explored.

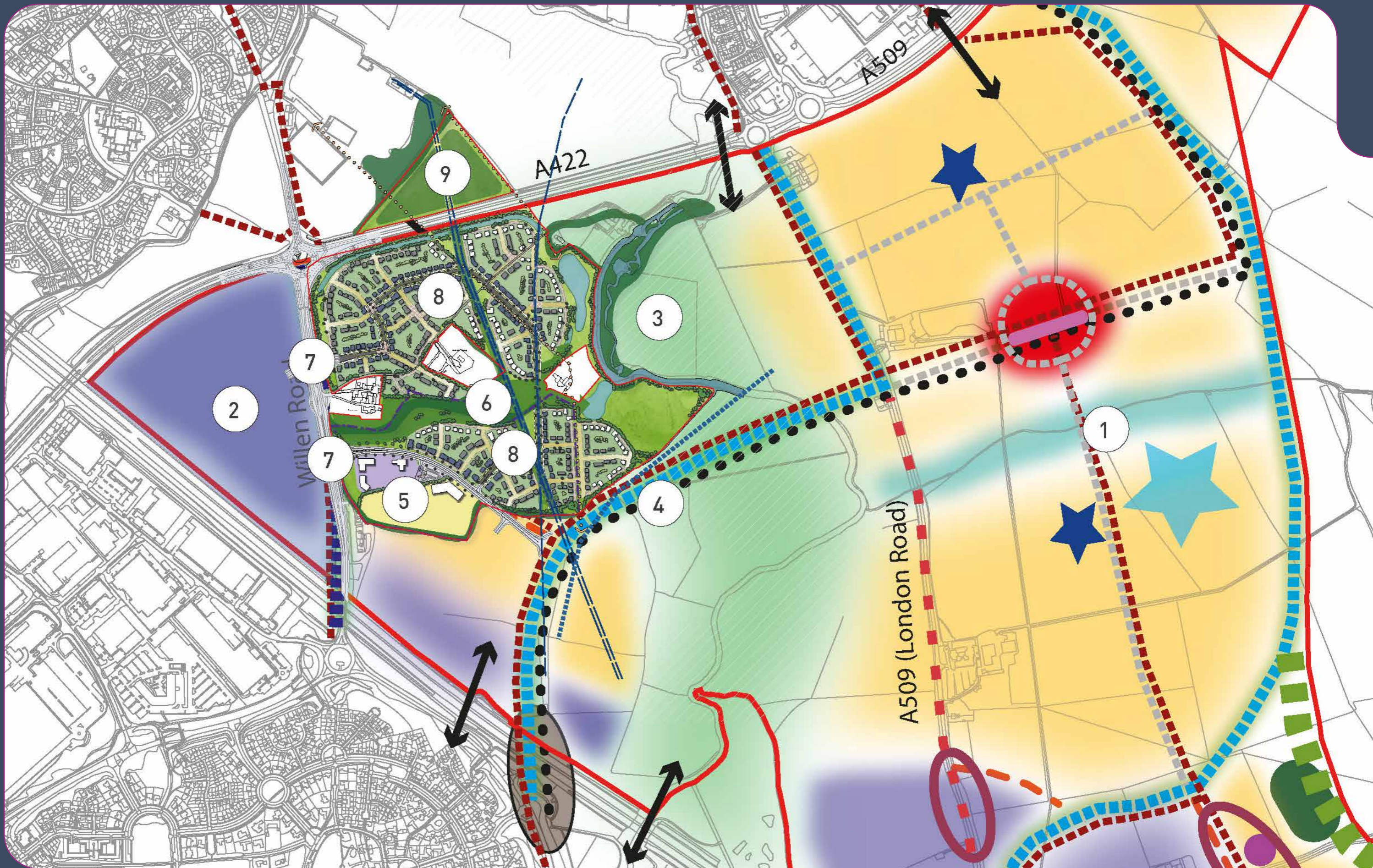
LOCAL CENTRE/ PRIMARY SCHOOL: SUE CONCEPT PLAN

ANALYSIS:

- Community uses are co-located albeit will be separated by the north-south greenway;
- The location shown does not consider the private land ownership that although is part of the high level strategic considerations, they are not part of this application;
- They are located in a visible location adjacent to the principal route enabling the local centre to benefit from some passing trade. However, trade from Willen Road and the employment area becomes more difficult without the user already knowing of the local centres location; and
- When considered in the context of the wider SUE, the community hub is not in a central position to serve the initial wider SUE phases. However, when viewing the application site in isolation they are central to application proposals.

INDICATIVE CONCEPT PLAN			
	Grid Road		Linear Park
	Existing Road to be Retained and Upgraded to Grid Road		Open Space Link
	Local Distributor Road		Green Buffer
	Fast Mass Transit		Indicative Location of Playing Fields
	Primary Residential Street		Potential Off-site Location of Playing Fields
	London Road to be Downgraded		Indicative Location of Secondary School
	Redway		Indicative Location of Primary School
	Park and Ride		Proposed Road Bridge
	Housing		Pedestrian/Cycle Connection
	Employment		New Junction (Detailed Location and Form to be Determined)
	Mixed Use Community Hub		SUE Boundary
	Local Centre		





PREFERRED DESIGN CONCEPT



- 1 Milton Keynes East Strategic Urban Extension Area (outside of application area)
- 2 Employment Area
- 3 River Ouzel and associated flood zone (predominately outside of application boundary). Riverside park to connect with wider SUE proposals and existing linear park located along eastern application boundary
- 4 Approximate route of Milton Keynes Grid Road
- 5 Proposed Local Centre and Primary School to serve western portion of SUE, located to enable visible presence alongside employment area and arterial routes (both existing and proposed)
- 6 East-west greenway connecting the employment to the Riverside Park
- 7 Proposed highways works to provide 2 no vehicular access points to the application site and enable access from the existing highways network to the wider SUE
- 8 Residential development parcels separated by east-west greenway providing the opportunity to create two distinct neighbourhood characters. Clear links, both vehicular and pedestrian, towards Employment, Local Centre and Primary School
- 9 Proposed Sports Pitch provision to serve the new and existing resident residents within the area

DESIGN PROPOSAL OVERVIEW

- 3.1 The plan shown opposite sets out the preferred design concept for the site and an over view of site proposals in the context of the SUE Concept Plan. Highlighting both how the proposals sit in the wider context and the areas of deviation.
- 3.2 The site proposals shares commonality with the SUE Concept Plan in all areas with the exception of 2no. access points and the location of the Local Centre/ Primary School.



THE DESIGN CONCEPT

5.18 The preferred Design Concept was then taken forward into the wider stakeholder consultation process.

5.19 The combination of the aforementioned layers creates clear development areas from which the initial concept for development can be formed and summarised as follows:

- **Site** - inherent site features, both natural and technical provide clear areas of development. For example the existing pipelines and their associated wayleaves create zones where either open space/roads need to be created. The linear form of these provide striking features through the development area that can be framed by the development proposals and aid in creating a memorable character feature that is unique to this area of the SUE. Furthermore the existing tree belt running through the centre of the site (north of the quarry area) provides the opportunity to create a central green link connecting Willen Road to the Riverside park to the east of the site. In combination, the pipeline wayleave areas and the central green link enable four quadrants of development to be created, which could be used as a base for future character area creation;
- **Movement** - being part of a wider allocation area and subject to the adopted Development Framework, points of access within the site have been well established as part of the wider strategic highways work being undertaken. The development concept and subsequent masterplan proposals will provide the appropriate areas of connection to the wider area and ensure future development is unprejudiced by ensuring the appropriate development responses at site boundaries;
-

- **Community** - the location of the local centre and primary school has been subject to numerous discussions with Officers. It has been proposed to locate these uses to the southern portion of the scheme where they are more closely linked to the wider SUE area and can make use of the less technically constrained land; and
 - **Place Making** - a hierarchy of streets and spaces, with defined recognisable character areas will add the final layers of place making to the proposals. The intention being not to create uniform development across the site, but to provide elements of commonality that tie the four quadrants of development together whilst allowing each to have its own distinct neighbourhood character.
- 5.20 The principles which have been developed provide a framework by which to create a distinctive place, with a consistent and high-quality standard of design. These principles have been derived from the site assessment, in conjunction with the delivery of a high-quality development which achieves the criteria set out within the NPPF, namely:

FUNCTION AND QUALITY

“...will function well and add to the overall quality of the area, not just for the short term but over the lifetime of the development”

(Para. 130(a), NPPF 2021)

- New development provides the opportunity to establish a distinctive identity to a place which, whilst having its own character, integrates with the surrounding built form and landscape context;
- Retention of the existing landscape features on the site where possible;
- Provision of a mix of uses which cater for the everyday needs of the new residents including work, education, leisure, recreation and retail activities whilst complimenting the existing surrounding local area;
- Provision of a range of house types, tenures and sizes in order to cater for choice and a variety of households;
- In-built ‘robustness’ – the ability of the development, including individual buildings, to adapt to changes such as use, lifestyle and demography over time;
- Provision of Sustainable Drainage systems to ensure that the development does not increase the risk from flooding in the area;
- New development at Willen Road will be designed to deliver the proposed residential, missed use, educational and open space uses associated with the western edge of the MKE SUE, and will represent value for money in terms of lifetime costs;
- Make efficient use of the site through proposing a development with an appropriate density; and
- It will be intuitive, comfortable, safe and easy for all to use irrespective of the environmental conditions identified within Section 2 above.

VISUALLY ATTRACTIVE

“...are visually attractive as a result of good architecture, layout and appropriate and effective landscaping”

(Para. 130(b), NPPF 2021)

- Provision of a clear hierarchy of connected spaces and places, including streets, accessible by a variety of users, which consider the design of the space as well as its function as a movement corridor;
- Integration of existing and proposed landscape features will help to soften the built form, particularly towards the eastern countryside edge of the development
- Minimise the impact of the development on the open countryside and surrounding context;
- New development will be set within a considered and attractive landscape setting; and
- Enrich the qualities of the existing place, with distinctive responses that complement the setting, respect the grain of the local area and acknowledge the established local character.

RESPONSE TO CONTEXT

“...are sympathetic to local character and history, including the surrounding built environment and landscape setting, while not preventing or discouraging appropriate innovation or change (such as increased densities)”

(Para. 130(c), NPPF 2021)

- Integration of the development into the new Milton Keynes East SUE, and the local area, particularly in relation to development block form, scale, height and massing;
- Consider how distinctive elements of the local vernacular and the best examples of local buildings relate to the space they enclose, and how this might be reflected within the proposals and can be used to inform the architecture of the proposed development;
- Consider carefully the specification of materials that respect/enhance the local vernacular;
- Respond to the existing site topography including the consideration of key views in and out of the site;
- Retention of the existing landscape features and habitats on the site; and
- Protection of existing and proposed residential amenity through the use of frontage development thereby enclosing rear gardens.

STRONG SENSE OF PLACE

“...establish or maintain a strong sense of place, using the arrangement of streets, spaces, building types and materials to create attractive, welcoming and distinctive places to live, work and visit”

(Para. 130(d), NPPF 2021)

- Allow the key characteristics identified within the Local Character Analysis (undertaken in Section 3) to influence the character of the development;
- Position key spaces & focal points where movement corridors converge to encourage activity and vitality;
- Creation of a development which allows ease of movement for all types of users and provides equal employment, social, community, leisure and retail activity opportunities for all;
- Consider how open spaces will best meet the recreational needs of the local community, thereby encouraging social interaction;
- Consider how the type and positioning of enclosures and soft landscape will clearly define the ownership of the space between buildings;
- Incorporate existing and proposed landscape features into the proposals, so as to enhance the richness and attractiveness of the streetscape; and
- Consider carefully texture, colour, pattern and durability of materials and how they are used.

ACCESSIBILITY

“...optimise the potential of the site to accommodate and sustain an appropriate amount and mix of development (including green and other public space) and support local facilities and transport networks”

(Para. 130 (e), NPPF 2021)

- Integration of the proposed development into the existing movement network of footpaths, cycleways, bus routes and vehicular routes, with new public transport provision with bus stops located within easy walking distance of the new dwellings;
- Provision of multiple access points into the development forming part of a permeable network of streets which assists in dispersing traffic (vehicular and pedestrian);
- Maximisation of the opportunities for alternative modes of transport to the car particularly walking, cycling and bus travel;
- Creation of a legible and permeable development, that is easy to navigate for all users, with a clear movement hierarchy providing easily recognisable routes, balancing the street as a space alongside its function as a movement corridor;
- Enhancement and extension of the existing public rights of way network as an integral part of the development, particularly facilitating sustainable access to the Town Centre and existing employment areas;
- Ensure a mix of appropriate residential tenures to further promote the economic and social success of the scheme; and
- Consider the potential for a variety of uses to be included within the development to promote its economic and social success, and to reduce the need to travel.

SAFE, INCLUSIVE AND ACCESSIBLE PLACES

“...create places that are safe, inclusive and accessible and which promote health and well-being, with a high standard of amenity for existing and future users; and where crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion and resilience”

(Para. 130 (f), NPPF 2021)

- Convenient, safe and direct access for all residents to the existing and proposed local services and facilities including schools, retail, community uses and employment opportunities;
- Provision of a variety of accessible public open spaces and recreation areas to meet the needs of the local community whilst encouraging social activity;
- Creation of a clearly defined public realm through the provision of continuous building frontage lines and variations in the methods of enclosure of private spaces;
- Consideration of the proposals in relation to the location of the buildings on the site, gradients, and the relationship between various uses and transport infrastructure, particularly for those with disabilities; and
- Control of access to private areas, particularly rear gardens and parking courts.

PRE-APPLICATION ADVICE AND DISCUSSIONS

“Local planning authorities should ensure that they have access to, and make appropriate use of, tools and processes for assessing and improving the design of development. These include workshops to engage the local community, design advice and review arrangements, and assessment frameworks such as Building for a Healthy Life. These are of most benefit if used as early as possible in the evolution of schemes, and are particularly important for significant projects such as large-scale housing and mixed-use developments. In assessing applications, local planning authorities should have regard to the outcome from these processes, including any recommendations made by design review panels.”

(Para. 133, NPPF 2021)

- 5.21 The proposed development was subject to pre-application discussions with Milton Keynes and Newport Pagnell Town Council’s through a series of meeting held over 2019 to 2021.
- 5.22 In summary, the following issues were discussed with the Council at pre-application stage:
- Integration of the application site with the wider SUE development
 - Coordination of the employment areas to the west with the access proposals
 - Potential local centre location options (see opposite)
 - Pedestrian and cycle access strategies
 - Key development frontages
 - Highways requirements
 - Density of development

LOCAL CENTRE OPTIONS

5.23 The following 3 options were presented to the LPA.

Option 1

- Parcel areas/shapes are unconstrained in terms of easements or rights of way
- Residential frontage to Willen Road provided.
- Local centre benefits from the trade generated by the employment area and passing trade from the link road along with local residents
- Co-located with primary school to reduce trips
- Dedicated pedestrian crossing points over link road to be provided

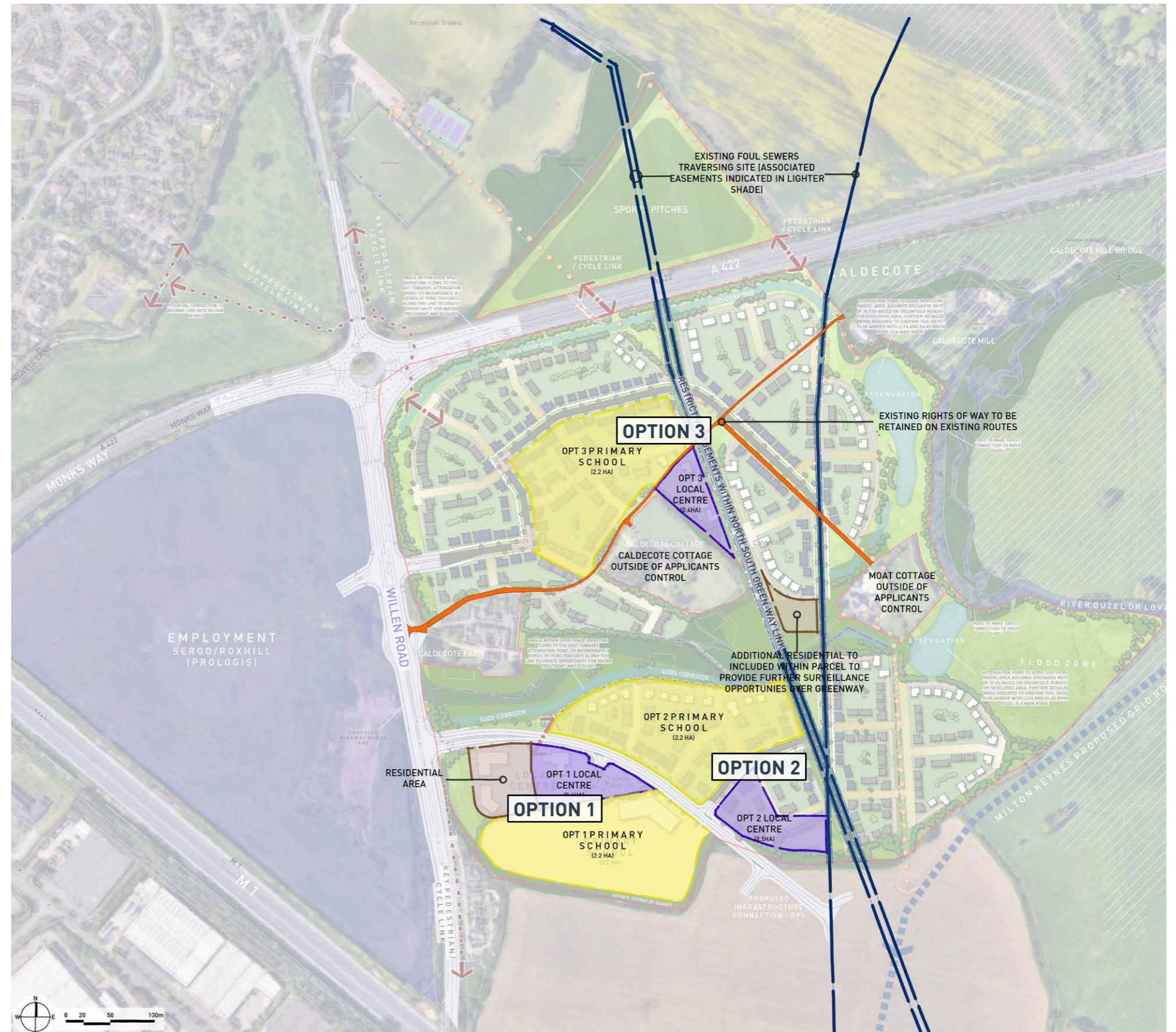
Option 2

- Parcel areas/shapes are unconstrained in terms of easements or rights of way
- Residential development to go to the south side of the link road
- Primary school location in general accordance with the location indicated at fig 4.7 Indicative development framework plan of the SPD
- Primary school set against the context of both green ways providing natural off road pedestrian routes
- Local centre benefits from the passing trade of the link road along with local residents
- Uses co-located to provide community hub

OPTION 3

- Both parcels areas/shapes constrained by restrictive easements and right of way
- Residential development to go to the southern sector parcels
- Right of way to remain unaltered and in situ will run along what is likely to be the rear of the primary school

5.24 Following discussions with officers it was felt that Option 2 presented the best balance of land uses.



KEY STAKEHOLDER AND COMMUNITY ENGAGEMENT PROCESS

“Local communities can play a vital role in achieving well-designed places and buildings and making sure there is a relationship between the built environment and quality of life. Communities can be involved in design processes through approaches such as co-design, design workshops and other engagement techniques, so that places and buildings reflect local community preferences, improve their quality of life and fit well into their surroundings.”

Para. 17, NDG 2021

- 5.25 Throughout Bloor Homes have carried out community and key stakeholder engagement and discussion to inform the proposals for Willen Road, Newport Pagnell.
- 5.26 The aim of the community engagement process has been to:
- Raise awareness of the project and design development;
 - Gather local knowledge to inform thinking, together with an understanding of the ambitions and key issues for the area’s future;
 - Be inclusive, accessible, transparent and engaging;
 - Offer a wide variety of ways for people to get involved;
 - Ensure a wide and representative range of views are heard;
 - Offer topic-specific events to enable thorough and dedicated discussions on key areas of interest;
 - Provide clear and thorough information, with experts and specialists on-hand to provide informed responses; and
 - Clearly communicate the feedback from the community, including any concerns, to the developer, the design team and relevant consultants.

PUBLIC CONSULTATION

- 5.27 Due to the impact of coronavirus, the applicant was unable to hold a formal, in-person, public consultation event; however, a virtual event was undertaken whereby local residents were invited to provide their comments in respect of the emerging plans for the site.
- 5.28 This took the form of a bespoke “Willen Road East” website which outlined key aspects of the proposals, including the latest version of the masterplan at that time. The website provided facility to provide comments on an online form. Residents were informed of the event via a leaflet drop to some 7,400 addresses in Newport Pagnell, Willen and Moulsoe. The event was also advertised by notice in the MK Citizen newspaper.

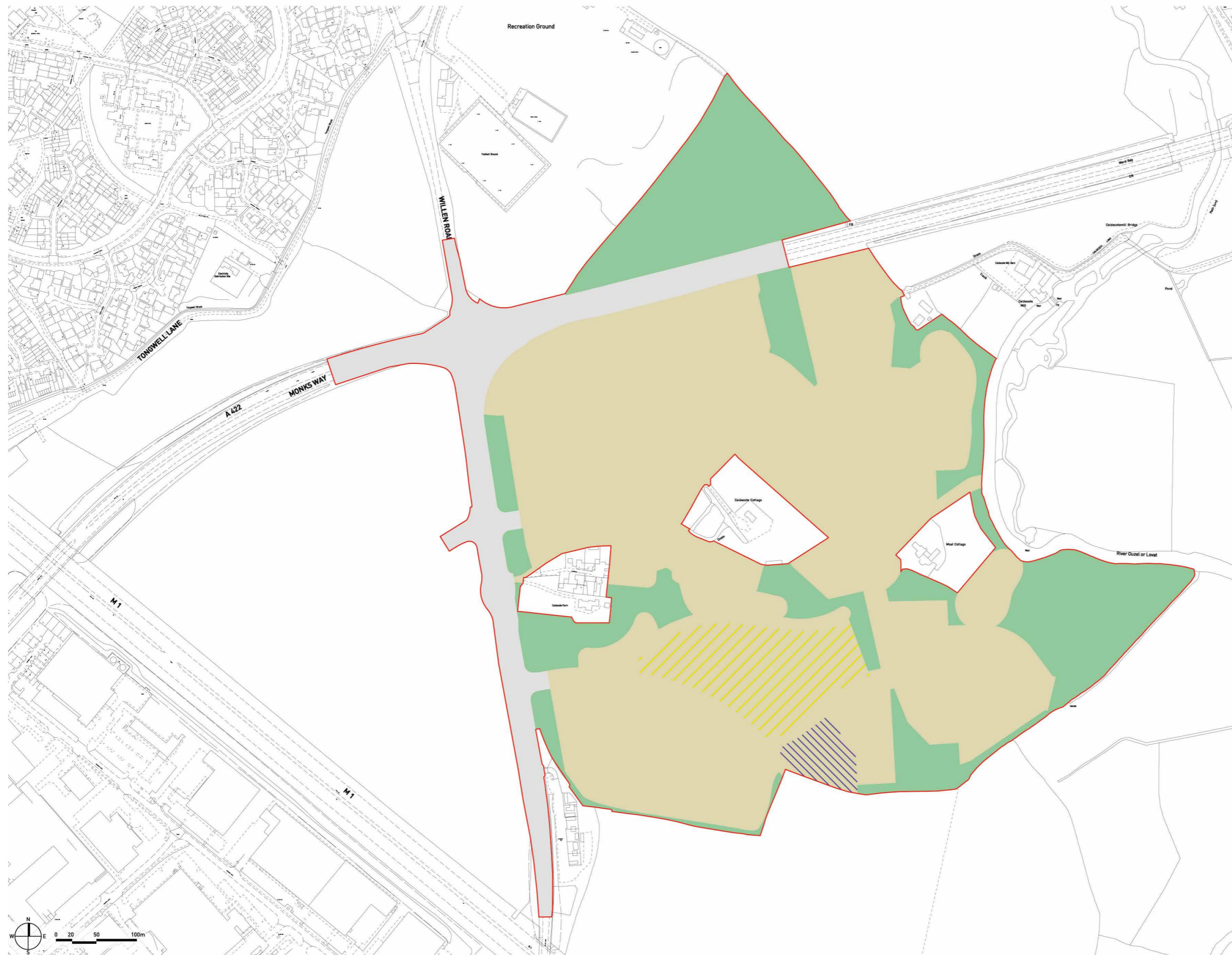
- 5.29 The comments received were then considered and, where appropriate, amendments made to the indicative master plan, which has resulted in the finalised scheme submitted as part of this application.
- 5.30 A summary of the responses received is provided below, and more information regarding public and stakeholder consultation is set out in the accompanying Planning Statement.

Neither support nor object – 32	Objection – 19	Support – 4
Plans seems to conflict re infrastructure proposals of adjoining site	Loss of green infrastructure	Welcome more affordable homes
Concerns about crossing A422 at street level rather than underpass	Issues with flooding	Glad to see more redways and hope they are fully connected with existing routes
Is there going to be a secondary school?	Too many homes impacting character of Newport Pagnell	Welcome more housing
Will there be a doctors?	Road crossings are in the wrong place to be useful and shouldn't be 'at grade'	
Roads need to be proper grid roads	Lack of information on plan re housing type, mix, linkages with surroundings developments etc	
Will there be a crossing from the southern housing to the school	School and local centre should be relocated	
Hope to see ground source heat pumps		
Need increased utilities and sewer capacity		
Site is part of flood plain, hopefully arrangements have been made to ensure no increase in off-site flooding		
Infrastructure needs to be built before any houses		
Hope to see more affordable units		

SUMMARY OF CHANGES MADE TO THE MASTERPLAN

- 5.31 The proposals for Willen Road have evolved throughout the iterative design process. The public consultation process has included public events, and a number of key stakeholder meetings.
- 5.32 The design of the proposed development has been an iterative process, informed by the consultation process, as well as emerging technical information.
- 5.33 Following the pre-application advice process and further, more detailed technical information the following subsequent changes to the masterplan were made:
- Redesign of the eastern development parcels to accommodate the revised sewer alignment, following geo-physical surveys to accurately locate them and the associated easement required;
 - Revised east-west green corridor through the centre of the site to accommodate revised drainage design;
 - Revised northern SuDs corridor;
 - Local centre parcel adjusted to better address the internal spine road, focussing the facilities more to the proposed primary school and less towards the Link Road as requested by planning officers;
 - Relocated NEAP to a more actively overlooked location in the west of the green corridor, freeing up space outside the school for informal public open space, suitable for outside a primary school
 - Revised design of development blocks to formally address the central east-west green corridor and provide active overlooking of central public open space; and
 - Revised more land efficient design of the A422 over bridge, allowing for more area for sports pitches, with the maximum playing area possible deliverable.





KEY

- SITE LOCATION**
- BUILT DEVELOPMENT**
To include residential development together with associated infrastructure, enabling engineering works (such as attenuation features), existing rights of access to adjoining properties, amenity green space, formal and informal play areas and landscaping
- PRIMARY SCHOOL**
Area to total up to 2.2 Ha, and is included within the total built development area
- LOCAL CENTRE**
Area to total up to 0.5 Ha, and is included within the total built development area
- HIGHWAYS INFRASTRUCTURE**
- GREEN INFRASTRUCTURE**
To include strategic open space, formal & informal open space, formal and informal play areas, sports pitches, landscaping, existing rights of access to adjoining properties and any other associated infrastructure/enabling engineering works (such as: attenuation features, A122 overbridge)

NOTE

Development area to include streets and spaces that will provide a movement network suitable for pedestrians/cyclists.
Development area & public open space areas to incorporate all necessary easements and/or PRoW/rights of access

LAND USE PARAMETER PLAN

DESIGN PROPOSALS

PARAMETER PLANS

- 6.1 The following elements form part of the formal application submission:
- Land Use Parameter Plan;
 - Access and Movement Parameter Plan;
- 6.2 The Parameter Plans and the associated wording set out in this chapter are to be 'fixed' as part of the Outline Planning Application. The plans, when read together will provide a framework for future, more detailed designs, and will define the type of development that can be brought forward at the Reserved Matters stage.
- 6.3 These parameters have been assessed in the Environment Statement forming part of this application, and provide a framework for future, more detailed designs. All information contained within this DAS should be read in conjunction with the additional supporting application pack.
- 6.4 The accompanying Illustrative Masterplan (presented later in this document) shows one way in which the development could be laid out in accordance with these parameters.

LAND USE PARAMETER PLAN

RESIDENTIAL

- 6.5 The proposals will provide land for residential development which will accommodate up to 800 dwellings in a range of types, sizes and tenures. The built development areas identified will include associated infrastructure; enabling engineering works (such as attenuation features); existing rights of access to adjoining properties; amenity green space; formal and informal play areas and landscaping alongside residential development.

GREEN INFRASTRUCTURE

- 6.6 The green infrastructure areas identified will include strategic open space; formal and informal open space; formal and informal play areas; sports pitches; landscaping; existing rights of access to adjoining properties and any other associated infrastructure/ enabling engineering works (such as attenuation features).

PRIMARY SCHOOL

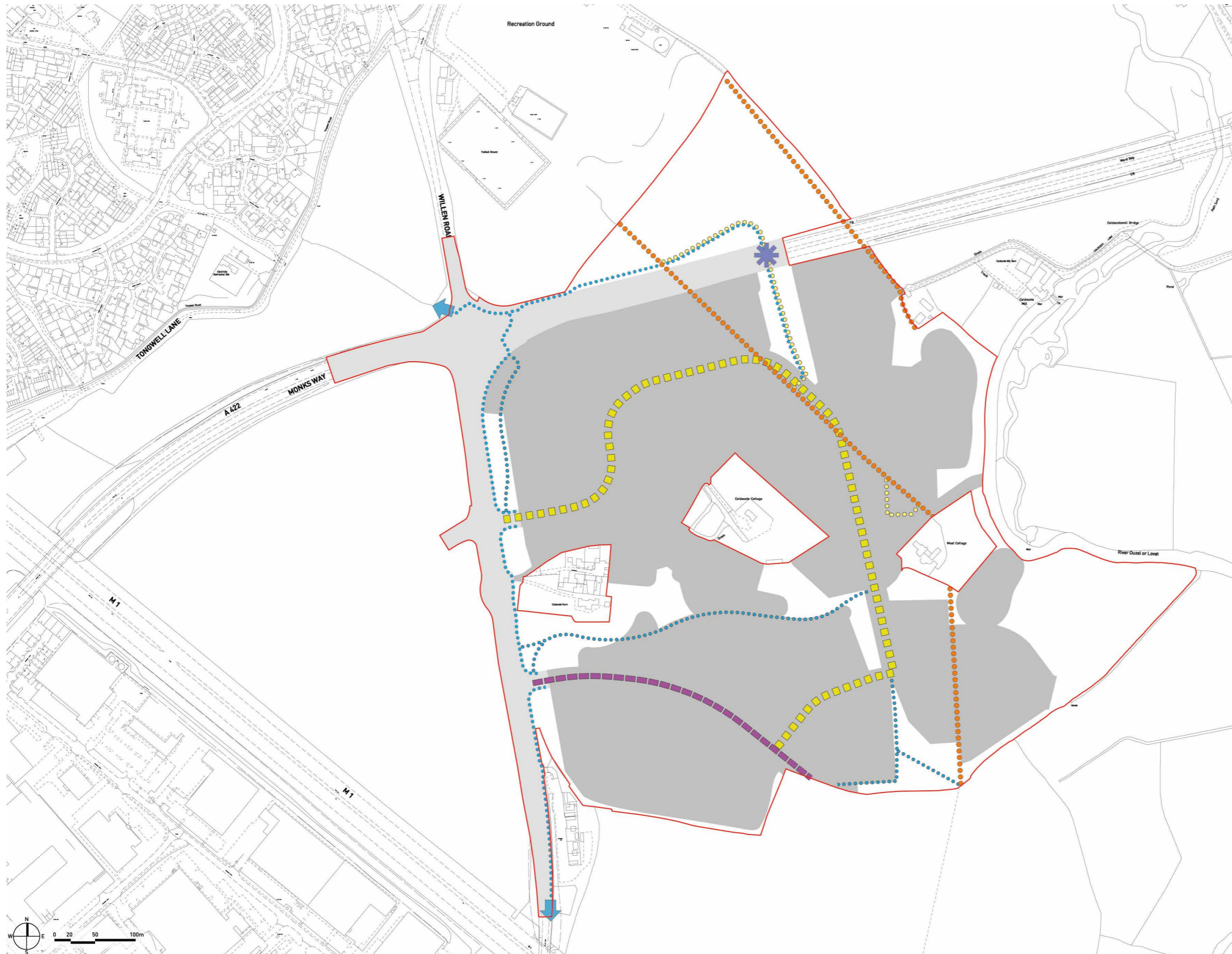
- 6.7 To the south of the site an area of up to 2.2Ha of land is provided within the total built development area for the delivery of a 2 form entry primary school to serve the development and wider SUE allocation.

LOCAL CENTRE










- 6.8 A local centre of up to 0.5 Ha will be provided within the total built development area to complement the existing local facilities and services in Newport Pagnell, and meet the requirements of Allocation SD12 and will include retail, health and community uses.
- 6.9 It is intended that the local centre within the site will complement the larger mixed use local centre provided south-east of the site in the wider SD12 Allocation
- 6.10 The local centre could provide the following uses:
- Children's day nursery (5,500 - 5,800 sqft);
 - Convenience store (4,000 - 4,500 sqft); and
 - 3 ancillary retail units (approxaimtely 1,000 - 1,500 sqft each)

UTILITIES

- 6.11 The land uses shown on the Land Use Parameter Plan will include easements and/or diversion routes for existing utilities as appropriate.



KEY

-  Site location
-  Proposed vehicular, cycle and pedestrian access point
-  Indicative alignment of proposed coal distributor road and redway
(As per MK East SUE Development Framework SPD, adopted March 2020)
-  Indicative alignment of spine road, including foot/cycleway
-  Pedestrian/cycle only access point
-  Proposed primary pedestrian/cycle route, where not alongside new streets
(Additional routes will be provided at the detailed application stage)
-  Existing PRoW footpath
-  Potential PRoW diversion route
-  Indicative location of proposed pedestrian/cycle overbridge

NOTE
Development area to include streets and spaces that will provide a movement network suitable for pedestrians/cyclists.

Development area & public open space areas to incorporate all necessary easements and/or PRoW/rights of access

LAND USE PARAMETER PLAN

ACCESS AND MOVEMENT PARAMETER PLAN

ACCESS POINTS

- 6.12 Vehicular access to the site will be provided via 3 new access points. Two access will be taken from Willen Road, in the west of the site, and one along the southern boundary (from the local distributor road), which will link the site to the wider SUE allocation.

VEHICULAR MOVEMENT ROUTES

- 6.13 The major vehicular movement routes are shown on the Access and Movement Parameter Plan (presented opposite) and aim to aid the creation of a legible development, setting out a clear hierarchy of streets.
- 6.14 The primary vehicular movement routes through the site (Local Distributor Road and Spine Road) will form a loop through the development enabling easy access to all areas of the development. These routes will be designed to be capable of accommodating a bus route through the site, with stops provided at regular intervals.
- 6.15 Secondary vehicular movement routes through the site will be accessed off the primary movement route and provide routes to the various residential parcels of the new development. They will provide direct access to properties.

PEDESTRIAN AND CYCLE MOVEMENT

- 6.16 Pedestrian access to the development will be provided from the 3 proposed vehicular access points, the 3 existing PRow access points, and an additional point of access located in the north-west of the site, which link with the existing Public Rights of Way (PRow) network.
- 6.17 Proposed MK Redway routes will run on both sides of the local distributor route. Footways and cycleways will be provided on either side of the spine road.
- 6.18 Within the north of the site a new overbridge will be provided, enabling easy pedestrian and cycle access to the existing and proposed sport pitches, located north of the A422, and access into Newport Pagnell.

EXISTING RIGHTS OF ACCESS

- 6.19 The existing rights of access to Caldecote Farm, Caldecote Cottage and Moat Cottage will be retained in situ.

SUPPORTING DESIGN STRATEGIES

- 6.20 The outline application plans are accompanied by a set of Supporting Design Strategies that illustrate how the development could be realised, in accordance with outline application. The supporting design strategies include:
- **Illustrative Masterplan;**
 - **Pedestrian and Cycle Access and Movement Strategy;**
 - **Vehicular Access and Movement Strategy;**
 - **Landscape Strategy;**
 - **Illustrative Landscape Masterplan;** and
 - **Indicative Phasing Strategy**
- 6.21 The strategies set out here are for illustrative purposes only, and do not form part of the formal outline submission. They are therefore subject to interpretation and discussion and should be read in conjunction with the parameter plans.



- KEY**
- Residential development (18.01 Ha)
 - School (2.20 Ha)
 - Mixed Use Areas (0.50 Ha)
 - Green Infrastructure (including attenuation) (17.71 Ha)
 - Spine road/movement corridor (2.33 Ha)
 - Existing retained rights of access (0.15 Ha)
 - Willen Road highways works (4.91 Ha)

RESIDENTIAL PARCEL AREAS

Residential parcel	Area (Ha)
R1	3.47
R2	4.48
R3	0.68
R4	2.86
R5	2.40
R6	0.61
R7	3.51
TOTAL	18.01

ILLUSTRATIVE LAND BUDGET PLAN

USES. MIXED AND INTEGRATED

NPPF chapters: 2, 5, 6, 7, 8, 12

“Well-designed neighbourhoods need to include an integrated mix of tenures and housing types that reflect local housing need and market demand. They are designed to be inclusive and to meet the changing needs of people of different ages and abilities. New development reinforces existing places by enhancing local transport, facilities and community services, and maximising their potential use.”

(Para. 109, NDG 2021)

7.1 The development proposals include the following:

RESIDENTIAL – UP TO 800 DWELLINGS (CLASS C3)

7.2 The development proposals provide approximately 18 Ha of residential development, achieving up to 800 dwellings.

AFFORDABLE HOUSING

7.3 It is proposed that 31% of the dwellings provided are to be affordable housing, in accordance with Policy HN2 “Affordable Housing” (Plan:MK 2016-2031). The precise mix of unit types, sizes and details of tenure arrangements are subject to consultation and agreement with the LPA and will be set out in the Section 106 Agreement.

PUBLIC OPEN SPACE AND GREEN INFRASTRUCTURE

7.4 17.7 Ha of accessible public open space has been provided within the proposals, in accordance with Policy L4 “Public Open Space Provision in New Estates” and Appendix C “Open Space Recreation and Facility Provision” (Plan:MK 2016-2031), and “Table 4.1 Open Space Standards” (MKE SUE Development Framework SPD March 2020).

7.5 The public open space will provide a multi-functional green infrastructure, retaining and enhancing existing tree and hedgerow planting wherever possible. The open space in the east of the site will form a major green corridor within the wider SUE site, with an accessible riverside park providing recreation space in close proximity to dwellings. Sports pitches will be provided to the north of the A422, adjacent to the existing Willen Road Sports Ground offering both existing and new residents of

7.6 Further details on the proposed landscape design are provided within the Landscape Strategy presented later in this document.

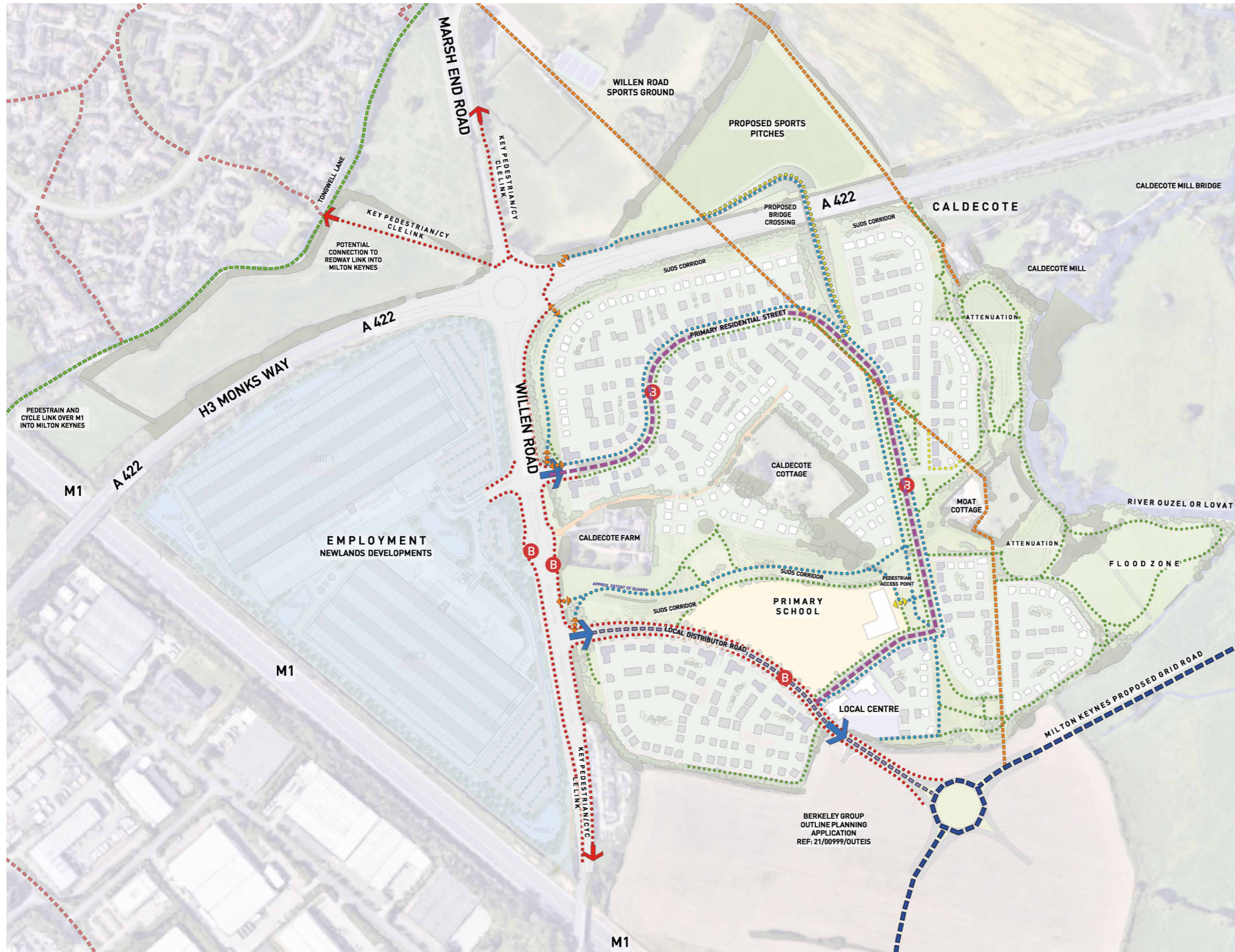
LOCAL CENTRE















7.7 Mixed-use areas provide a good basis for building communities that, in turn, will provide support to the proposed facilities. The outline application proposes a local centre within the south of the site, accessible to both the existing and proposed communities.

7.8 Totalling 0.5 Ha the local will provide a mix of small scale uses. The intention is to cater for day-to-day needs of the new local community, with a larger mixed use community hub located in the wider SUE area.

7.9 Space for the following uses could be accommodated:

- Children’s day nursery (5,500 - 5,800 sqft);
- Convenience store (4,000 - 4,500 sqft); and
- 3 ancillary retail units (approximately 1,000 - 1,500 sqft each)



- KEY**
-  Proposed vehicular, cycle and pedestrian access point
 -  Existing MK redway
 -  Proposed MK redway
 -  Existing PRoW
 -  Other route with public access (shown on OS mapping)
 -  Potential PRoW diversion
 -  Proposed pedestrian/cycle routes
 -  Proposed pedestrian routes
 -  Pedestrian access point to school
 -  Proposed pedestrian/cycle access points
 -  Proposed bus stop (indicative locations shown)
 -  MK Grid Road (Fig 4.2 MK East SUE Development Framework SPD) forming part of Berkeley Group Outline Application
 -  Local Distributor Road (MK Type 5)
 -  Spine Road

PEDESTRIAN AND CYCLE ACCESS AND MOVEMENT STRATEGY PLAN

MOVEMENT. ACCESSIBLE AND EASY TO MOVE AROUND

NPPF chapters: 8, 9, 12

“Patterns of movement for people are integral to well-designed places. They include walking and cycling, access to facilities, employment and servicing, parking and the convenience of public transport. They contribute to making high quality places for people to enjoy. They also form a crucial component of urban character. Their success is measured by how they contribute to the quality and character of the place, not only how well they function.”

Para. 75, NDG 2021)

- 8.1 The Indicative Masterplan shows the disposition of land uses and the proposed structure for movement within the development. A well-connected movement network, accessible by all users, is proposed which helps to ensure that all areas of the development will be accessible, easy to navigate, safe and secure. The proposed access and movement strategy will focus on the delivery of the following elements which are in accordance with the objectives of national and local planning policy:
- Proposed access points;
 - Proposed pedestrian and cycle movement network;
 - Street hierarchy;
 - Street typologies; and
 - Parking strategy.
- 8.2 The location of the development, adjacent to the existing and established community is a positive characteristic which has been maximised through the provision of direct and attractive pedestrian routes.
- 8.3 The proposed access strategies set out here clearly define the main routes and help to achieve a permeable layout. The Access and Movement Strategy Plan, presented opposite, shows the proposed structure for movement within the development

PEDESTRIAN AND CYCLE ACCESS STRATEGY

- 8.4 The development of an integrated pedestrian/cycle network within the site is seen as a key part of the transport infrastructure for the site. Pedestrians are led into the site from links created between areas of existing and proposed development, including the new vehicular access points.
- 8.5 Cycle use is encouraged through the high degree of permeability within the layout. With local facilities located nearby and low vehicular speeds proposed within the development, cyclists will therefore find it safe and convenient to use the streets for cycling.
- 8.6 The following measures to provide accessibility by foot and cycle are proposed and illustrated, where appropriate, on the Access and Movement Strategy Plan:
- Pedestrian access to be provided in the north-western corner of the site to connect to the Marsh End Roundabout and the Redway and pedestrian / cycle crossing to be delivered in conjunction with the Employment Site.
 - A new Redway route along the eastern side of Willen Road between the Tongwell Roundabout and the Northern Access, connecting to the Redway being provided between the Northern Access and the Marsh End Roundabout in conjunction with the Employment Site. It is proposed that Willen Road (S), south of the proposed Southern Access will be a key pedestrian / cycle link to the Tongwell Roundabout.
 - To improve pedestrian and cycle connections to the north towards Newport Pagnell it is proposed to provide a new footbridge with ramps and steps to accommodate pedestrian and cycle movements over the A422 along the route of Footpaths 007 and 014.
 - Other pedestrian / cycle connections are to be provided from the eastern side of the Development Site, connecting to the wider MKE-SUE to the east.
- Provision of an off-road shared use formal foot/cycleway (minimum 3m width) running adjacent to the Primary Residential Street;
 - Provision of circulatory pedestrian routes (minimum 2m width) through the site, offering pedestrians easy access to development and a choice of routes away from the Primary Residential Street;
 - Integration and where necessary and appropriate diversion of existing PROW within the site;
 - Where possible pedestrian links will be suitable for use by disabled people;
 - Particular attention will be paid to ensure surface material quality and sufficient active overlooking, to provide a sense of safety and security for users; and
 - To ensure that vehicular movement corridors do not become a barrier to pedestrian/cyclist movements crossing points will be defined where appropriate, to enable all users to cross safely.

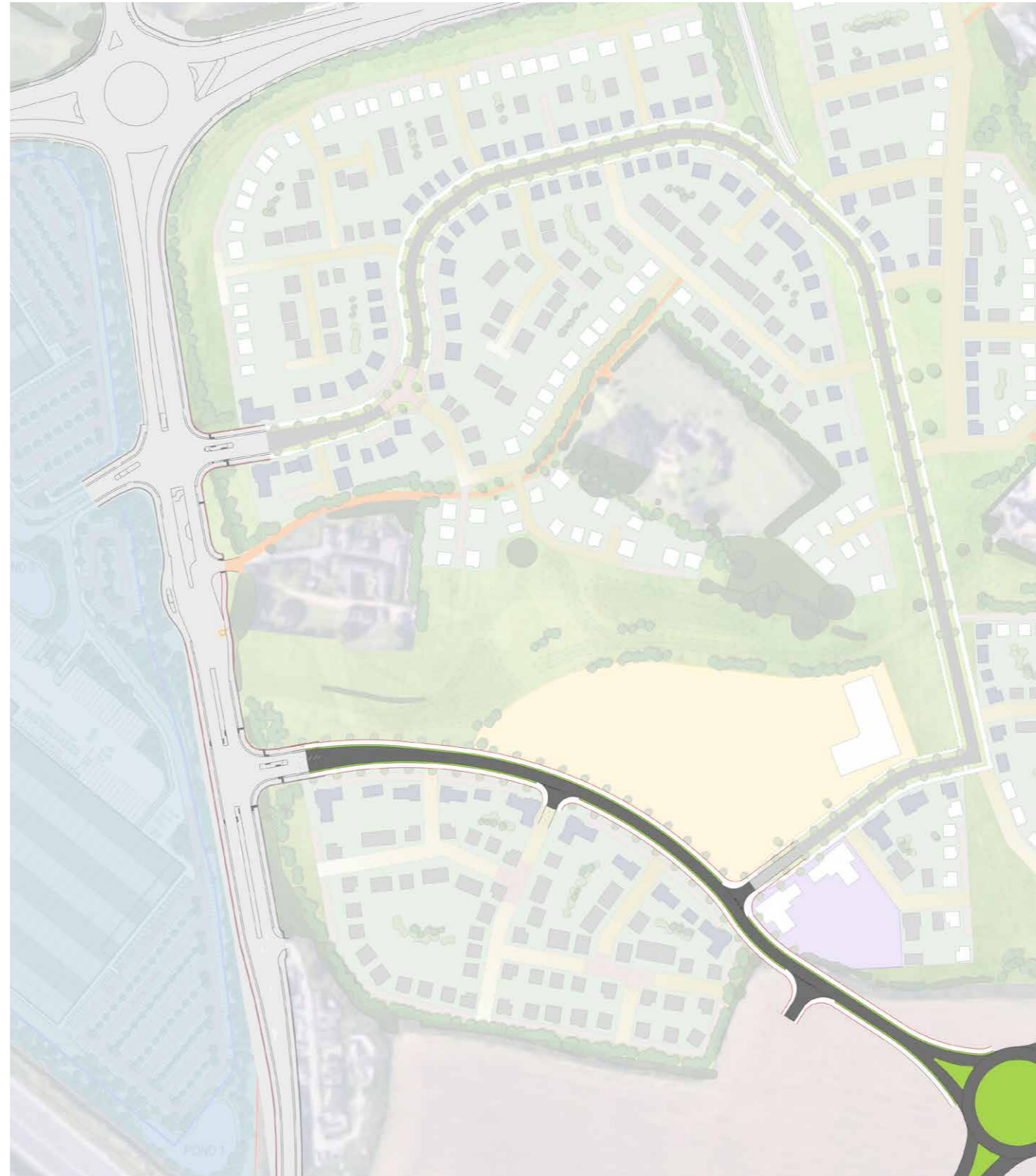


PROPOSED VEHICULAR ACCESS POINTS









- 8.7 Vehicular access has been carefully designed to coordinate with the local highway network improvements proposed as part of the committed development of Caldecote Farm Employment, located on the opposite side of Willen Road and the wider MKE-SUE to the east/southeast of the site.
- 8.8 The proposed vehicular access points serving the development proposals are indicated on an accompanying plan including:
1. Willen Road northern access – this will tie into the proposed signal-controlled junction to serve the Caldecote Farm Employment site in the form of a signal controlled crossroad junction. The junction improvement will include the provision of a pedestrian/cycle crossing across the site access road arm of the junction;
 2. Willen Road southern access – located approximately 190 meters south of the northern access, this will be a new signal-controlled junction which will include the provision of pedestrian/cycle crossings on all arms; and
 3. A southern access point will provide a direct link from the south east of the site to the wider MKE-SUE. It will connect directly with the new Grid Road/Redway that is to be delivered by the wider highways works. The precise form of this connection is part of ongoing highways design work.

OFF-SITE HIGHWAY MITIGATION

- 8.9 A number of wider highways mitigation and improvement measures are proposed as part of the application. These include Willen Road to be widened to a two-lane dual carriageway and will then taper down to a two-way single carriageway on the approach to the M1 overbridge, in accordance with the aspirations of the MKE SUE to upgrade Willen Road to a grid road and in conjunction with the Employment Site, the speed limit on Willen Road between Tongwell Roundabout to the south to the entrance to Newport Pagnell (Marsh End Road) to the north, will be reduced from the national (60mph) to 40mph.
- 8.10 For further details on the proposed access strategy reference should be made to the Transport Assessment contained within the ES, submitted in support of this Outline Application.





- KEY**
-  MK Grid Road (Fig 4.2 MK East SUE Development Framework SPD) forming part of Berkeley Group Outline Application
 -  Local Distributor Road (MK Type 5)
 -  Spine Road
 -  Principle Street (MK Type 6)
 -  Major Street (MK Type 7)
 -  Street (MK Type 8)
 -  Shared Street (MK Type 10)
 -  Shared Drive - Non Adopted (MK Type 11)

STREET HIERARCHY PLAN

STREET HIERARCHY

- 8.11 A clear hierarchy of streets is proposed creating an integrated movement network. Variation in the street types proposed aids in the creation of a legible and permeable development, whilst also providing for, and encouraging pedestrian and cycle movement, and delivering necessary vehicular connections.
- 8.12 Streets will be designed as key aspects of the public space, the nature and form of which will vary according to their connectivity, function and location within the development proposals. The development proposals have been influenced by “Manual for Streets 1 & 2”, which encourages designers to move away from standardised prescriptive measures and to adopt a more innovative approach, in order to create high- quality places for all users, ages and abilities.
- 8.13 Incorporating nature, particularly tree planting, within the streets is a key principle in the design of new developments. Tree-lined streets has been given a priority in the latest edition of the NPPF, stating that:
- “Trees make an important contribution to the character and quality of urban environments, and can also help mitigate and adapt to climate change. Planning policies and decisions should ensure that new streets are tree-lined, that opportunities are taken to incorporate trees elsewhere in developments (such as parks and community orchards), that appropriate measures are in place to secure the long-term maintenance of newly-planted trees, and that existing trees are retained wherever possible. Applicants and local planning authorities should work with highways officers and tree officers to ensure that the right trees are planted in the right places, and solutions are found that are compatible with highways standards and the needs of different users”.*

(Para 131, NPPF 2021)

- 8.14 The development and internal road network will be designed encourage low vehicular speeds (circa 20mph) and streets will be defined by the building layout, so that buildings and spaces, instead of roads, dominate the street scene. The design will promote safe walking and high permeability through the site and aims to limit the potential for anti-social behaviour.
- 8.15 The proposed street hierarchy recognises the need to combine the function of the street as a movement corridor, alongside its placemaking function. The importance of each of the street types in terms of its movement and place function varies within the hierarchy.
- 8.16 Within the site the Spine Road will provide the main vehicular access route into the development from Willen Road to the west of the site. Forming a loop through the site the Spine Road will increase the permeability of the development and enables easy access to dwellings from the primary access point.
- 8.17 Lower category roads feed off the Spine Road, serving smaller areas of residential development. Areas of hard paving are broken up with soft landscaping, creating attractive routes within the development.
- 8.18 Street lighting will be designed in conjunction with street tree planting to ensure safe and acceptable levels of lighting throughout the development.

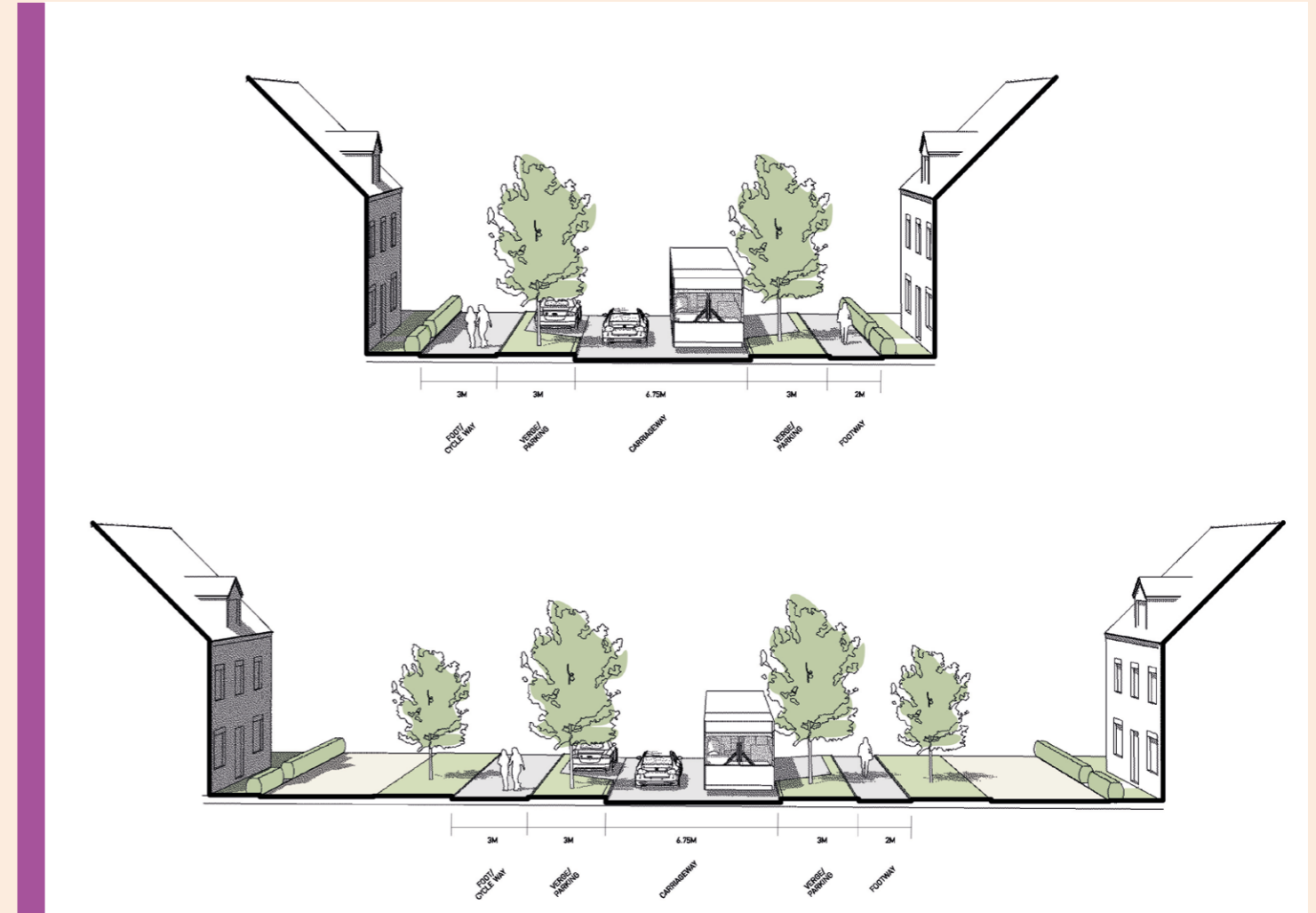
STREET TYPOLOGIES

- 8.19 The following street typologies are proposed for inclusion within the proposals:
- Local Distributor Road
 - Spine Road
 - Principle Street
 - Major Street
 - Street
 - Shared Street
 - Shared Drive (Non-adopted)

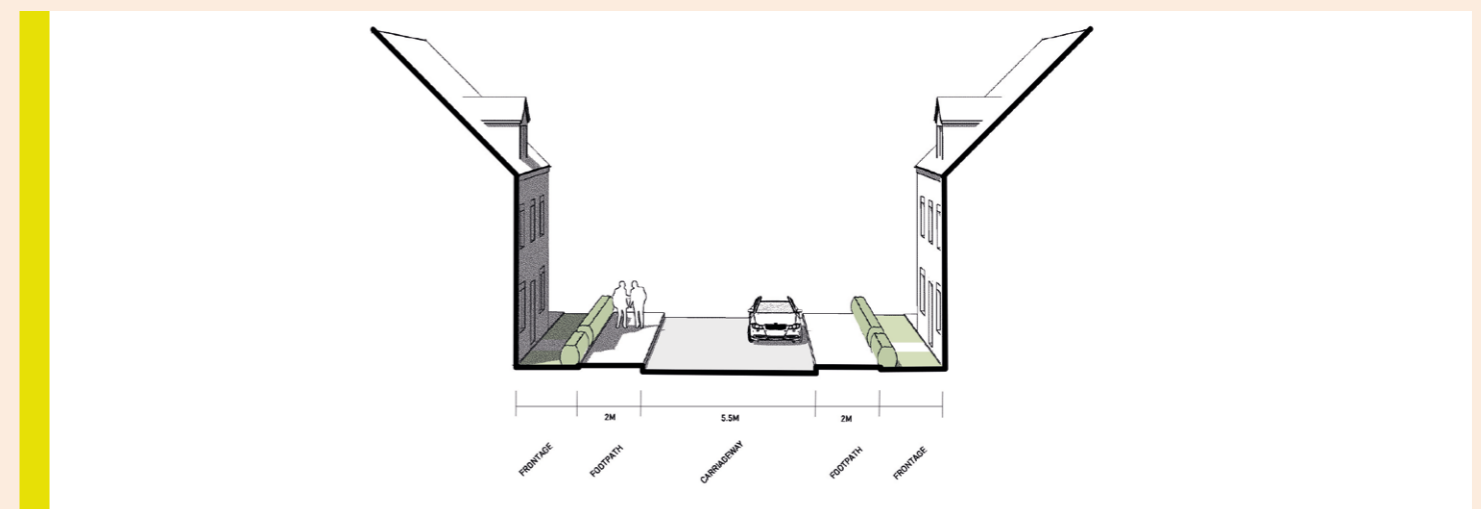
LOCAL DISTRIBUTOR ROAD



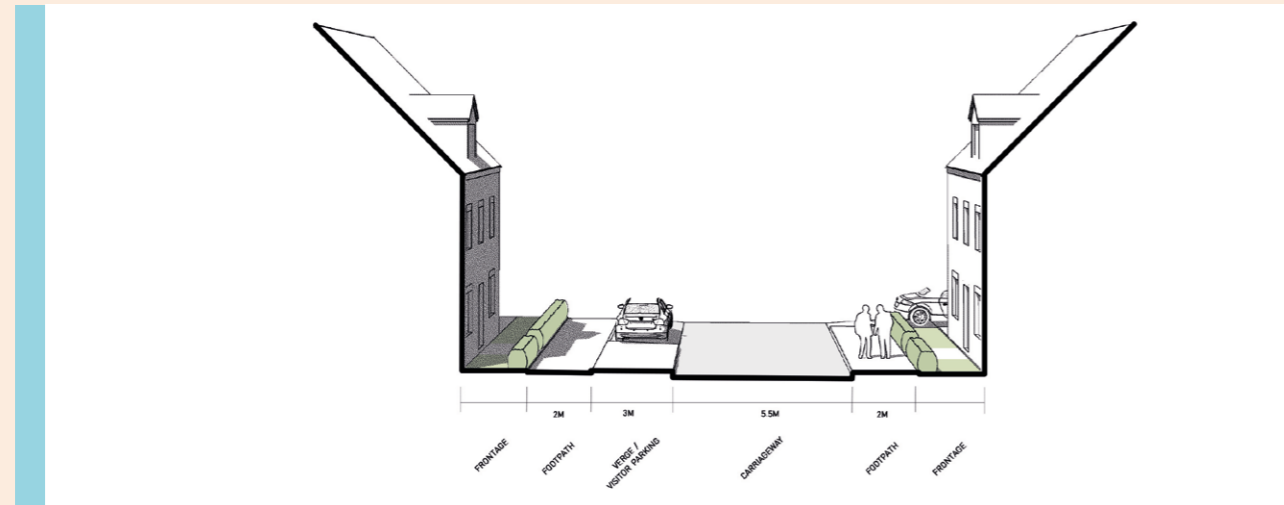
SPINE ROAD



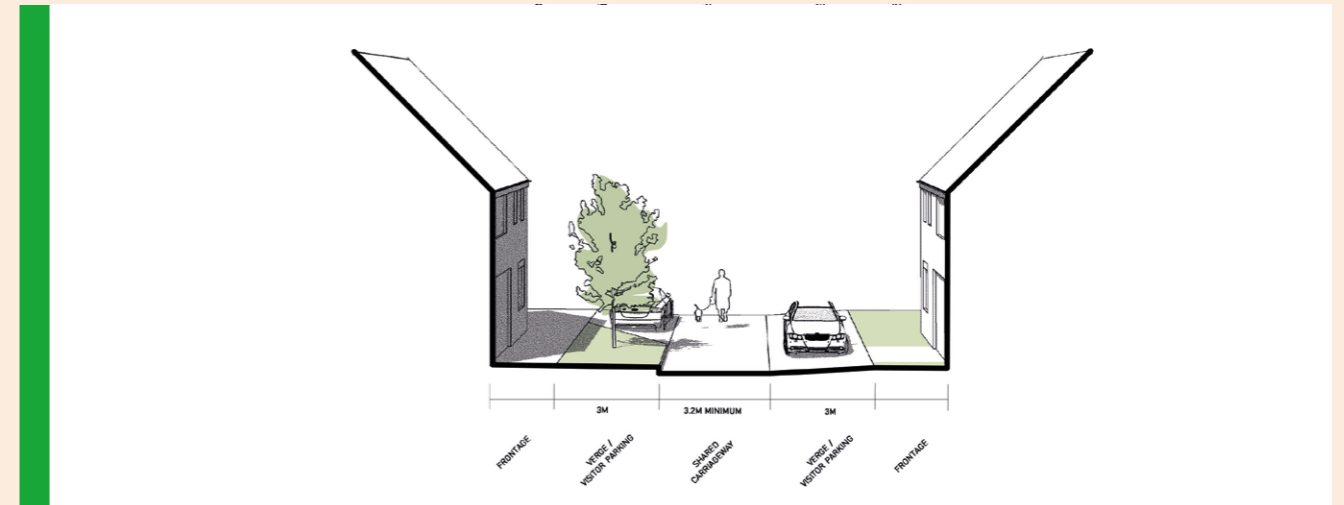
PRINCIPLE STREET



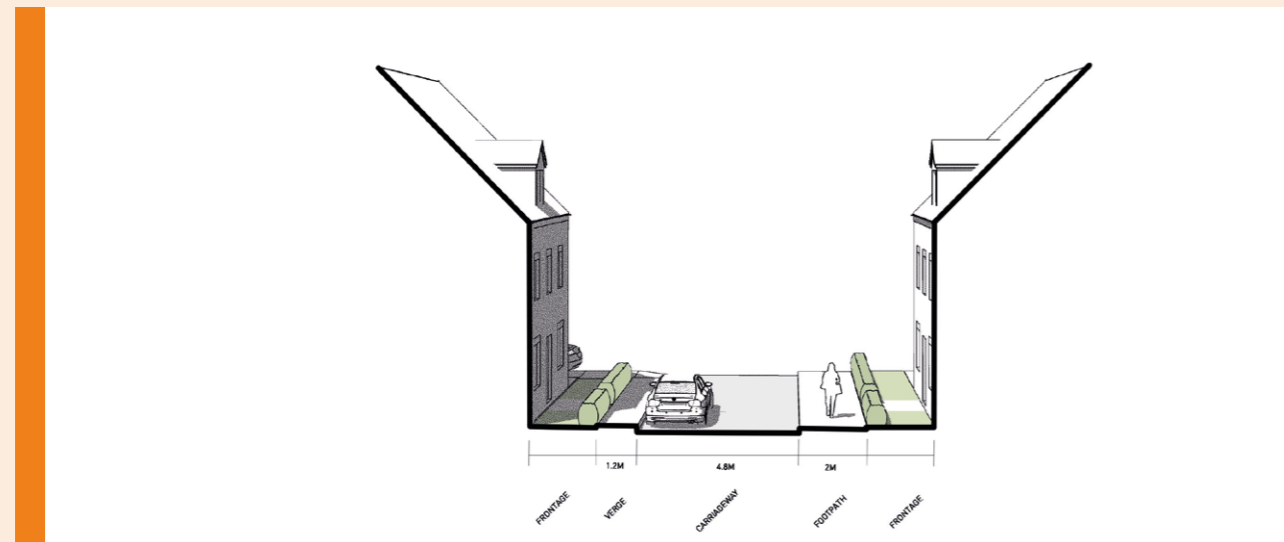
MAJOR STREET



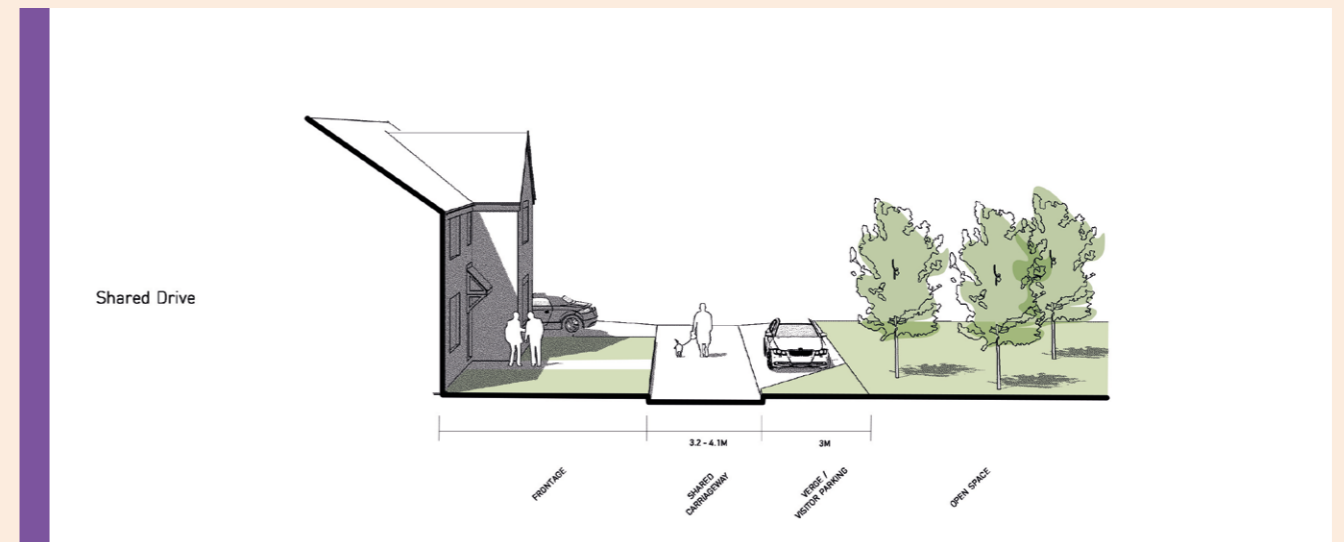
SHARED STREET

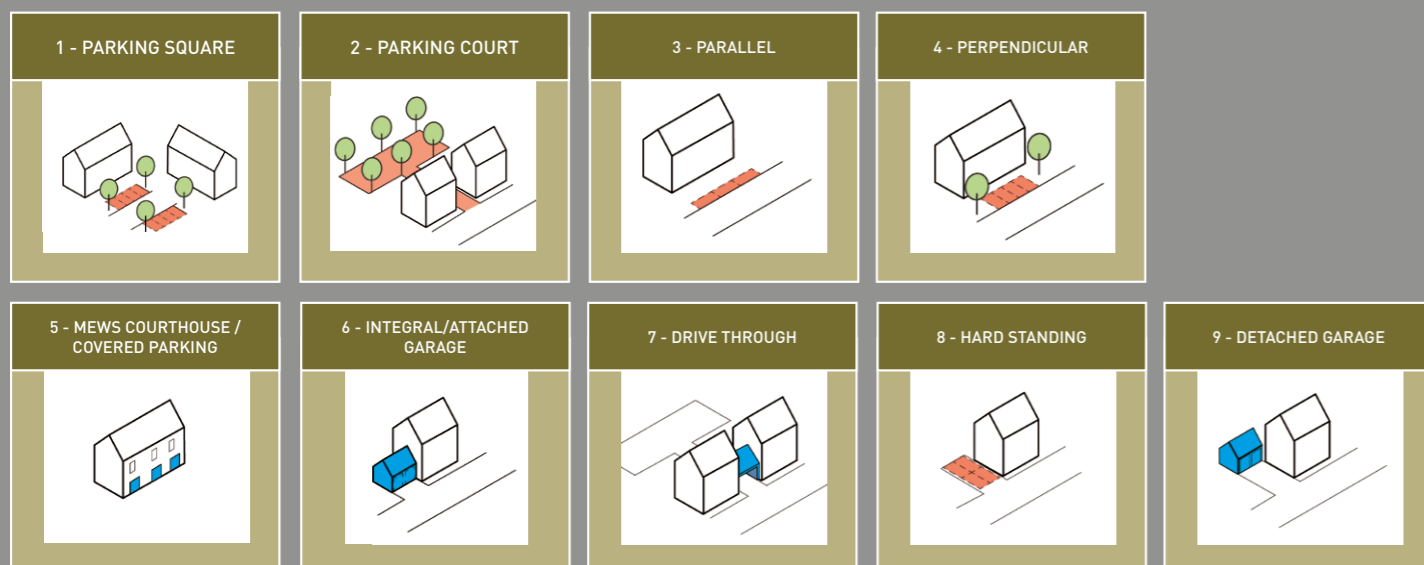


STREET



SHARED DRIVE (NON-ADOPTED)





	Name	Type	Allocated?	Description	Comments
1	Parking square	Off-plot	Optional	Group(s) of parking bays located adjoining the main carriageway providing convenient access to dwellings	Convenient access to parking. Good surveillance from neighbouring properties. To be used sparingly, and in conjunction with high-quality landscaping scheme
2	Parking court	Off-plot	Optional	Group(s) of parking bays and/or garages located within a shared courtyard (see further guidance in 6.32)	Generally limited to up to 5 dwellings Maximum 15 parking space if serving apartments. Tandem bays within parking courts should be avoided
3	Parallel	On-street	No	Parking can be located within the carriageway. Accessed directly off the road.	Easily accessible. Maximum of 4 bays in a row Suitable for visitor parking only, bays to be unmarked
4	Perpendicular	On-plot/ Off-street	Optional	Parking located perpendicular (90°) to the carriageway. Accessed directly off the highway.	Can be marked or unmarked. Easily accessible. Generally suited to streets where speeds are kept to a minimum. Maximum rows of 4 bays in a row without landscaping break
5	Mews courthouse/ covered parking	On/off-plot	Yes	Terraced garages with residential uses above. Serving dwellings in the vicinity only	Allows enhanced natural surveillance over parking and offers efficient use of land.
6	Integral/attached garage	On-plot	Yes	Private garage adjoining the dwelling, often allowing access directly to house.	To be set back from public domain to allow parking in front. Convenient access to dwelling. Could be attached to neighbouring property and allows for room above.
7	Drive through	On-plot	Yes	Parking bay and/or garage access through a covered arch on the street.	Helps avoid car dominated street scene whilst providing secure on-plot parking. To serve maximum of 2 dwellings only
8	Hard standing	On-plot	Yes	Parking bay located next to dwelling.	Can be located against the back edge of public domain or set back to allow additional parking in front. Can be joined to neighbouring parking bay.
9	Detached garage	On-plot	Yes	Private garage often located next to dwelling. Garages to be set back from prominent frontages. Careful design required to mitigate impact of parked cars on the street scene.	Must be set back to allow parking in front. Can be joined to neighbouring garage.

PARKING

- 8.20 Parking will be designed in line with the current guidance contained within Manual for Streets and MKC Parking Standards SPD (January 2016).
- 8.21 Parking should be provided in a location that is both convenient and well overlooked. It should be designed to be as unobtrusive to the street scene as possible, with screening provided by the use of hedges and planting, where appropriate.

RESIDENTIAL PARKING STRATEGY

- 8.22 Allocated parking will predominantly be provided on plot, within the curtilage, either to the front or side of dwellings, with individual bays and/or garages set back from the building line, to allow ease of access to dwellings.
- 8.23 Allocated residential parking will be provided at the minimum rates as set out in the table below:

Unit size	Allocated spaces per unit	Additional unallocated visitor spaces per unit
1 bedroom dwelling	1	0.33
2 bed flat	1	0.75
2 bed dwelling	2	0.25
3 bed dwelling	2	0.5
4+ bed dwelling	2	0.5

- 8.24 The site currently falls within MKC Parking Standards Accessibility Zone 4 “Rural Areas”, however, due to the size and scale of the MKE-SUE development the Zone 3 Urban areas rate has been applied. The development will not be a standalone rural development, and it forms part of the much larger SD12 allocation area, thus applying the rural area rate would be lead to the provision of excessive visitor parking and encourage the use of less sustainable modes of transport.
- 8.25 Vehicle/pedestrian visibility splays of 1.5 x 1.5m/2m x 2m (from the back of highway to the side of driveway, assuming a 2.4m car width) should be incorporated where parking spaces abut the back edge of footway or the highway boundary.
- 8.26 Where parking is provided on street (via parallel or perpendicular bays) it should be carefully designed to be typically no more than 4 spaces appear in a row, and areas of landscaping and/or planting should be used to break up the appearance.
- 8.27 Disabled parking will be provided in accordance with the appropriate standards.

REAR PARKING COURTS

- 8.28 The use of rear parking courts should only be used in exceptional circumstances. Where parking courts are utilised, they should serve no more than 6-12 parking spaces, and should be designed to include good levels of natural surveillance and opportunities for active overlooking. Detailed guidance regarding the design of rear parking courts is given in para 4.30 – 4.38 of the MKC Parking Standards SPD (January 2016).

ELECTRIC VEHICLE CHARGING

- 8.29 Suitable infrastructure and ducting to enable the future installation of electric vehicle charging points should be provided to dwellings.
- 8.30 Rapid and fast EV charging points will be provided within the local centre and school.

TANDEM PARKING

- 8.31 Independently accessible on-plot parking spaces are preferred.
- 8.32 If tandem parking spaces are proposed as part of the detailed design stage then an additional (in addition to the standard visitor requirements), convenient, on-street parking space must be provided at a rate of 1 space per every two dwellings that have tandem parking (or any similar layout where the spaces are not accessed independently).

GARAGES

- 8.33 Garages will not count towards the allocated parking spaces for a dwelling.
- 8.34 Parking spaces located in front of garages must be a minimum of 6m long.

CYCLE PARKING

- 8.35 Cycle parking spaces for individual dwellings should be provided within the curtilage of the dwelling, at the rates set out in the table below, in accordance with MKC Parking Standards SPD (January 2016):

Unit size	Allocated spaces per unit	Additional unallocated visitor spaces
1 or 2 bedrooms	1	20 per 40 dwellings
3+ bedrooms	2	

- 8.36 Where cycle parking is to be accommodated within garages then these should be of an appropriate size to ensure that there is room for both car and cycle parking.
- 8.37 For apartments secure cycle parking should be provided in a communal facility.

VISITOR PARKING STRATEGY

- 8.38 Unallocated visitor parking will be provided at the rates set out above.

NON-RESIDENTIAL PARKING STRATEGY

- 8.39 Car and cycle parking for the shops and services associated with the proposed local centre, and the employment areas will be provided as set out in MKC Parking Standards SPD (January 2016).



- KEY**
- Built form - Main Street
 - Built form - Neighbourhood Core
 - Built form - Green Edge
 - Local Centre
 - Primary School
 - Green infrastructure
 - Adjacent employment development
 - Proposed vehicular, cycle and pedestrian access point
 - Existing rights of access to be retained
 - Proposed MK grid road (Fig 4.2 MK East SUE Development Framework SPD)
 - Proposed MK local distributor road (Fig 4.2 MK East SUE Development Framework SPD)
 - Primary access/link road
 - Existing PRoW
 - Potential PRoW diversion
 - Proposed pedestrian/cycle access points
 - Existing MK redway
 - Existing pedestrian/cycle route (leisure/traffic free/quiet route)
 - Proposed MK redway
 - Proposed pedestrian/cycle links
 - Proposed bus stop (indicative location shown)
 - Flood Zone 3

ILLUSTRATIVE MASTERPLAN

BERKELEY GROUP
OUTLINE PLANNING
APPLICATION
REF: 21/00999/OUTEIS

BUILT FORM. A COHERENT PATTERN OF DEVELOPMENT

NPPF chapters: 8, 9, 11, 12

“Built form is the three-dimensional pattern or arrangement of development blocks, streets, buildings and open spaces. It is the interrelationship between all these elements that creates an attractive place to live, work and visit, rather than their individual characteristics. Together they create the built environment and contribute to its character and sense of place.”

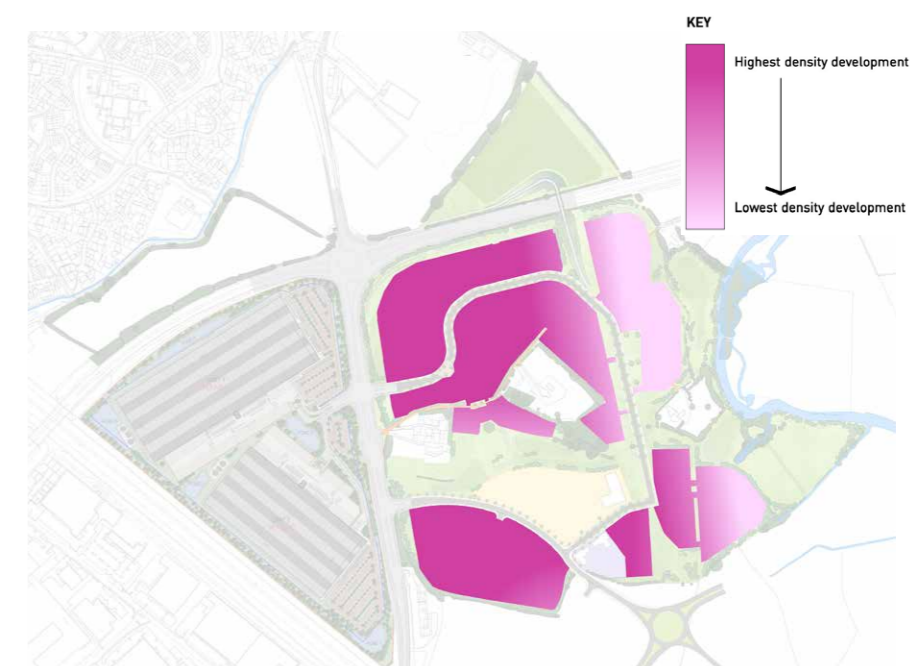
(Para. 61, NDG 2021)

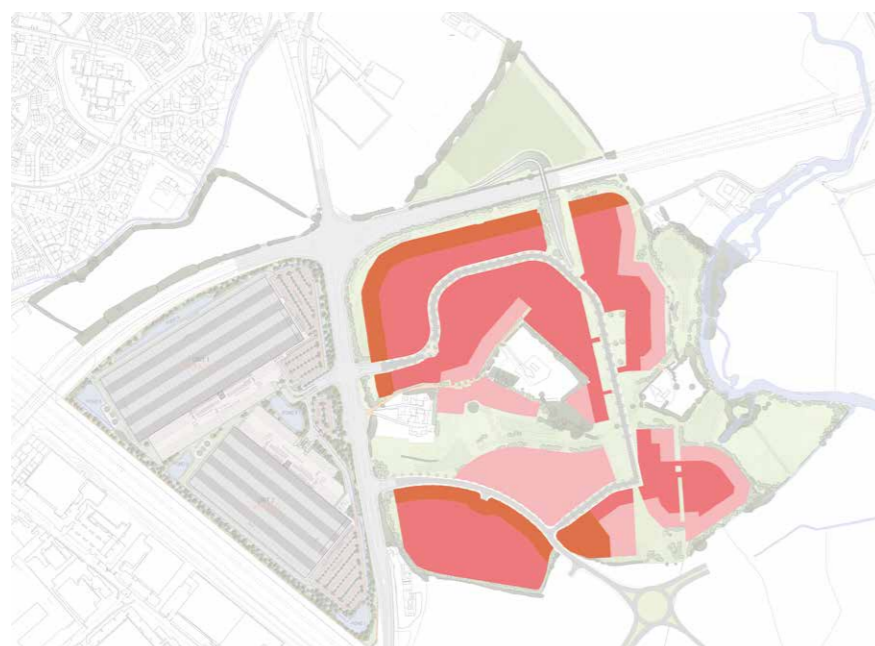
- 9.1 The design solution for the site reflects the variety in townscape form that can be seen in the locality and in particular the area surrounding the site.
- 9.2 The arrangement of the built form at Milton Keynes East creates a network of attractive street and spaces including:
- A wider main street corridor emphasising the primary vehicular access taken from the Willen Road North access and looping through the site to eventually connect with the Willen Road South access and the MK grid road;
 - A secondary vehicular access taken from Willen Road;
 - Provision of additional informal pedestrian and cycle routes, reflecting key desire lines, and enhancing the existing PRow network;
 - A number of safe pedestrian routes to the Primary School and Local Centre giving pedestrians a choice of routes and journey times;
 - Access to development blocks will be provided via a legible network of streets with a clear hierarchy;
 - The design of the development proposals is based on the principle of perimeter blocks that enclose back gardens, providing a strong frontage to the public realm and ensuring active frontages overlook streets and spaces wherever possible;

- 9.3 The relationship between existing properties within the SUE area, their rights of access and proposed development is a key element of the masterplan. Development will seek to create appropriate relationships with these existing properties and incorporate their rights of access as seamlessly as possible;
- A series of focal spaces will be accommodated within the proposals aiding legibility;
 - Green infrastructure is a key organising element of the masterplan, aiming to ensure a site-specific identity is created. Development has been shaped by the proposed new central green link, running through the centre of the site;
 - Existing tree and hedgerow planting has been retained wherever possible within the proposals;
 - The provision of wider key green infrastructure links will help to improve existing habitats on-site, as well as catering for biodiversity enhancements;
 - A new local centre to cater for the local communities needs. A larger mixed use community hub is provided with the wider SUE area;
 - Development is set back from the site boundaries, enabling the creation of a softer development edge, and a sensitive landscaped transition from built form to the riverside park that is to be created adjacent to the River Ouzel;;
 - The central area of open space will be linked to additional small-scale spaces via green corridors, creating a network of new spaces and places that vary in function and character;
 - The development will provide active frontages to proposed areas of public open space, landscape planting and areas for formal play, providing natural surveillance and opportunities for active overlooking; • and
 - Larger/taller or by merit of materials and detailing, more distinct recognisable landmark buildings are located in key locations.

DENSITY

- 9.4 The development will ensure the efficient use of land, yet is reflective of the surrounding area, helping to assimilate the proposals into the surrounding areas.
- 9.5 The development will allow for the formation of differing densities across the development; including higher density development towards the western boundary, and lower densities towards the open space areas of the site.





- KEY**
- 2 storey (up to 9.0m ridge)
 - 2.5 storey (up to 11.0m ridge)
 - 3 storey (up to 13.0m ridge)

BUILDING HEIGHTS (SCALE)

- 9.6 The height and massing of the proposed development varies across the site according to the nature of the public realm to be created. The majority of residential development will be 2-storey, reflecting the surrounding built form of the local area
- 9.7 Variety in the heights and massing of the residential buildings will be achieved through the use of a range of house types and sizes, ranging from smaller 1 and 2 no. bed apartments, through to larger 4 and 5-bedroom detached houses.
- 9.8 The use of an increased in storey heights (up to 2.5-storeys) could be used where it would appropriate to aid legibility and provide articulation within the street scene, or to define the following key elements of the proposals:
- Along the Spine Road;
 - Development overlooking the central play area; and
 - Terminating key views along the spine road
- 9.9 Development fronting Willen Road and the A422, and that located within the local centre will be up to 3-storeys aiding legibility and providing an element of distinctiveness with the scheme.
- 9.10 Dwellings will be up to 2-storeys where the proposed development backs onto existing properties adjoining the site boundary, in order to respect the privacy and amenity of the existing dwellings.

CONTINUITY AND MASSING

- 9.11 Key development frontages, such as those overlooking areas of public open space and following the primary movement route, will be particularly prominent and critical to the appearance of the development. Particular attention will be paid to the massing and architectural style of these buildings, so that they contribute positively to the quality and character of the new development. These frontages should be designed as a composition, with consideration also given to the spaces they adjoin, in order to provide a cohesive approach to these prominent positions.
- 9.12 However, as this is an edge of the town location, some streets could incorporate a more open aspect, with elevations set back behind deeper private planted frontages. This would reflect the suburban character of more recent late 20th century development in Newport Pagnell, and the more traditional and rural characters of the villages of Willen and to the south of the site.

HOMES AND BUILDINGS. FUNCTIONAL, HEALTHY AND SUSTAINABLE

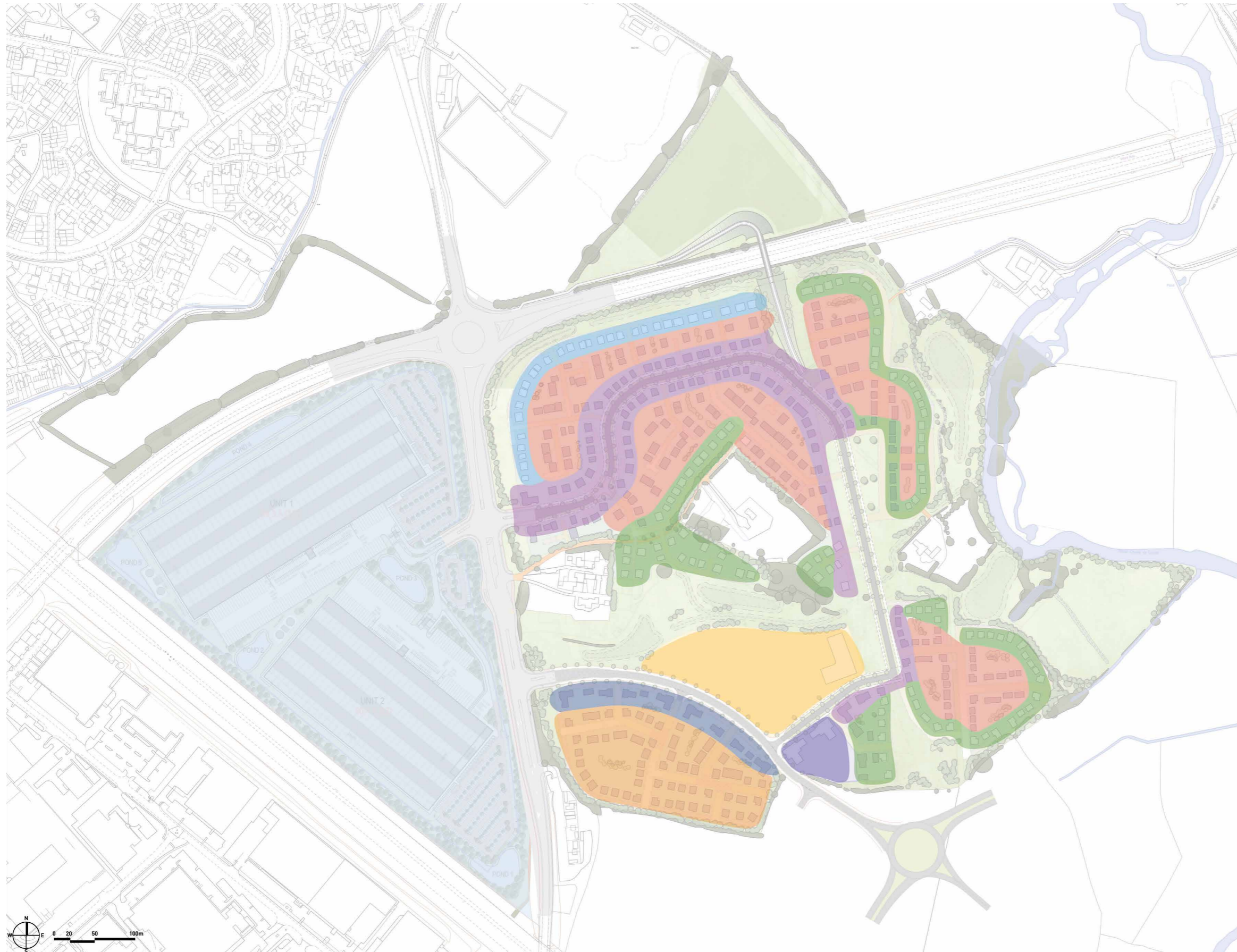
NPPF chapters: 8, 12

“Well-designed homes and buildings are functional, accessible and sustainable. They provide internal environments and associated external spaces that support the health and well-being of their users and all who experience them.”

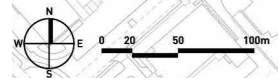
(Para. 120, NDG 2021)

- 10.1 The proposals for Milton Keynes East: Willen Road will comprise a distinctive character and a strong sense of place, informed by important site features and the existing valued qualities of the local area.
- 10.2 The proposals aim to create a place that has a healthy, comfortable and safe internal and external environment.
- 10.3 House frontages could be carefully designed with generous windows from habitable rooms, clearly defined and attractive front doors and planting to act as buffer between the pavement and window.
- 10.4 Internal habitable rooms could have high levels of natural daylight and connect well to gardens and terraces.
- 10.5 Affordable housing will be well-integrated with a tenure blind approach taken to the design of units, so there is no discernible difference between private and affordable dwellings.
- 10.6 Refuse storage could be convenient with access to rear gardens with the requisite internal storage, with refuse collection within acceptable operative carry distances of 25m.





- KEY**
- Willen Road Frontage
 - Contemporary Avenue
 - Main Street
 - Green Edge
 - Core Family Housing
 - Contemporary Family Housing
 - Primary School
 - Local Centre



CHARACTER AREAS PLAN

IDENTITY. ATTRACTIVE AND DISTINCTIVE

NPPF chapters: 8, 12, 15, 16

“The identity or character of a place comes from the way that buildings, streets and spaces, landscape and infrastructure combine together and how people experience them. It is not just about the buildings or how a place looks, but how it engages with all of the senses.”

Para. 49, NDG 2019

11.1 Character areas are a useful way of helping assimilate the design proposals within its surroundings, whilst providing a continuity of themes across the development and helping to generate a sense of place. Each character area will contain its own individual design components which aid in making it distinct from other areas. These components of character include the built form elements referred to earlier will include built form principles, and in addition consideration of changes in building height, building setbacks, landscape treatments, architectural detailing and materials.

11.2 The site has been divided into 8 proposed character areas each with a clearly defined character relating to the site’s context and surroundings. The following pages describe how the character areas should be designed in such a way to help create a varied and diverse townscape. The character areas are detailed below as follows:

- **CA1:** Willen Road Frontage
- **CA2:** Contemporary Avenue
- **CA3:** Main Street
- **CA4:** Green Edge
- **CA5:** Core Family housing
- **CA6:** Contemporary family housing
- **CA7:** Primary School
- **CA8:** Local Centre

11.3 A summary of the proposed residential character is set out on the following pages. The design of these character areas will form part of the future detailed design stage.

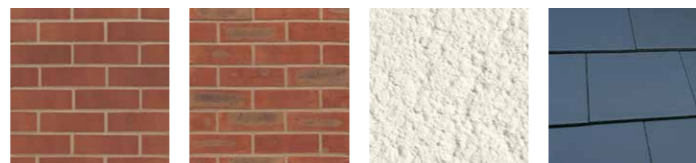
CA1

WILLEN ROAD FRONTAGE

- 11.4 The Willen Road Frontage will have a semi-formal character that is defined by development overlooking the northern and western site boundaries.
- 11.5 Forming a key frontage to the development it will be the first visible and most prominent area of housing as you approach the site from Newport Pagnell and Milton Keynes.

UNIQUE AND DEFINING CHARACTERISTICS

- General use of increased storey heights (2.5 – 3 storeys) will help to define this key frontage
- Predominant use of outward facing semi-detached dwellings and a consistent approach to the architectural detailing will help the creation of a more formal character
- The use of a consistent built form and building line will aid rhythm in the street scene and aid noise mitigation
- The traditional architectural style will provide a sensitive transition between the SUE to the existing development of Newport Pagnell, with dwellings finished in red brick, with contrasting red-multi brick or render used to highlight key buildings
- Front gardens are generally shallower; residential boundary treatments will be defined with hedgerows, to provide a consistent approach





CA2

CONTEMPORARY AVENUE

11.6 The Contemporary Avenue will have a semi-formal character that is defined by development overlooking the Link Road and the central east-west green corridor. The Contemporary Avenue forms a major link from the site to the wider SUE allocation.

UNIQUE AND DEFINING CHARACTERISTICS

- Development will have a contemporary architectural approach
- General use of increased storey heights (2.5 – 3 storeys) will help to define this key route through the scheme
- Predominant use of semi-detached and detached dwellings fronting the street
- Less variation in architectural details to aid a sense of rhythm along the street-scene and a formal character
- The contemporary architectural style will complement the local centre and primary school, whilst providing a sensitive transition to more traditional residential character in Newport Pagnell to the north of the site
- Façades will comprise a variety of buff brick, render and weather boarding
- Front gardens are generally shallower; residential boundary treatments will be defined with hedgerows, with metal railing used to enhance the contemporary approach





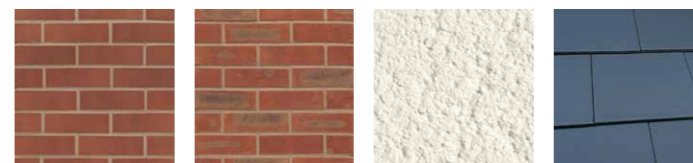
CA3

MAIN STREET

11.7 The Main Street will have a semi-formal character that is defined by key views along the street to retained trees and areas of public open space, as well as key buildings, encouraging movement along the route.

UNIQUE AND DEFINING CHARACTERISTICS

- The Main Street will meander gently through the development with less horizontal deflection than other secondary and tertiary routes. This alignment aids the creation of terminating views to key buildings and allows for an area of public open space to be located along the route
 - Use of increased storey heights (2.5 storeys) will help to define the routes
 - Predominant use of larger semi-detached and detached dwellings
 - Less variation in architectural details to create a sense of rhythm and aid a more formal character along the street-scene
 - Traditional materials: red brick with slate coloured roof tiles
 - Consistent approach to materials throughout the character area
 - Front gardens are generally shallow
 - Private frontages will be defined by hedgerows
- 11.8 At key junctions, the use of focal trees with a different form or size, or foliage colour shall be used to help denote the change with the character along the Main Street, or to denote the threshold of a character area in conjunction with the focal building.





GREEN EDGE

11.9 Green Edges will be defined by a generosity of space, the use of lower densities and views across areas of open space. Provision of large development blocks that are structured in accordance with the topography:

UNIQUE AND DEFINING CHARACTERISTICS

- The use of meandering street forms ensure views to green space within the development and to the wider countryside to the south of the site
- The central east-west green corridor is a key adjoining space, this will aid the creation of a green and verdant character. It is linked to the green corridors that adjoin the edge of the site
- Lower density development will aid a generosity of green space
- Dwellings will be limited to 2 storeys, aiding the creation of a less formal and a rural character, respecting views across the site from the river parkland
- Predominantly detached dwellings with some semi- detached properties
- Dwellings will be located in larger plots
- Predominant use of red brick, render and weather boarding façades, with a variety of roofing materials
- Front gardens are generally larger
- Residential boundary treatments will be defined with hedgerows
- The landscape design shall replicate the less formal rhythm of the built edge. Hedgerows shall provide transitions between the public and private spaces to provide a verdant edge with the use of native species. Overall the landscaping shall be traditional in style reflecting the looser street structure.





CA5

CORE FAMILY HOUSING

11.10 The Core Family Housing forms the residential heart of the development.

11.11 The character area generally comprises a semi-formal and ordered arrangement of development blocks, served by secondary and tertiary streets that are structured to provide views out to the Green Edges and open space beyond:

UNIQUE AND DEFINING CHARACTERISTICS

- Predominant use of semi-detached or terraced units, located within smaller plots
- Use of increased storey heights (2.5 storeys) to aid a sense of enclosure and to mark key buildings
- Use of more suburban building forms
- Facades finished in a variety of bricks and render, with key buildings defined by an increase in storey heights and the potential use of enhanced detailing or a change in materials
- Potential for more formal residential boundary treatments such as railings to define shallower frontages
- Use of hard landscaping and street tree planting to break up areas of frontage parking
- In style and form the residential landscaping shall compliment the semi-formal, suburban style of the neighbourhood core architecture and built form
- Hedgerows shall be used to define the private boundaries of key dwellings, with the ornamental planting species chosen for their sensory qualities as well as maximising floristic diversity for nectar and pollen resource.





CA6

CONTEMPORARY FAMILY HOUS

11.12 The contemporary family housing is focussed in the south-west of the site and generally comprises a semi-formal and ordered arrangement of development blocks and tertiary streets.

UNIQUE AND DEFINING CHARACTERISTICS

- The use of long and straight streets aids a more formal character
- Predominant use of semi-detached or terraced units, located within smaller plots
- Use of increased storey heights (2.5 storeys) to aid a sense of enclosure and to mark key buildings
- Use of more suburban building forms, will provide an identifiable contrast to the Main Street and Green Edges
- Façades will be finished in a variety of bricks and weatherboarding, with key buildings defined by an increase in storey heights rather than a change in the materiality
- Simple architectural detailing and the choice of materials will emphasise the contemporary approach, with the use of flat roof porch canopies and dormers providing consistency within the character area





PRIMARY SCHOOL

- 11.13 Providing a focus for the new community the primary school is located in the south of the site, adjacent to the east-west green corridor and immediately north of the local centre. The school building will respect the privacy and amenity of adjacent existing residential development and will be limited to 2 storeys in height.
- 11.14 The education facilities should provide a safe and welcoming environment for children, providing a flexible space where pupils can learn, socialise and support each other.
- 11.15 Specific architectural guidelines are not set out for the educational facilities; however, it is expected that the buildings will be high-quality and sustainable, reflecting the principles of the wider residential development.
- 11.16 The final building designs are subject to discussion and approval with MKC Education Authority.

UNIQUE AND DEFINING CHARACTERISTICS

- The proposed primary school will be prominently located in the centre of the development, adjacent to the Spine Road.
- Park and stride design to be implemented, with limited parent drop off spaces encouraging travel by sustainable modes.
- Vertical timber cladding, buff brick, standing seam roof, render and/or weather boarding could be used to reference the traditional vernacular.
- Contemporary architectural approach to accentuate the landmark nature of the building.
- Incorporating elements such as asymmetrical roof forms and the repetitive use of gables will add interest to the street scene.
- The school should be designed to meet Milton Keynes requirements and early engagement with officers is encouraged.





CA8

LOCAL CENTRE

- 11.17 Providing a range of facilities and services the Local Centre will feature a mix of uses and visitors, creating a vibrant heart for the new community. The local centre
- 11.18 Defined by an increase in storey heights the Local Centre offers the opportunity for a more contemporary architectural approach to be developed. The built form will help to define the key links along the Link Road and the north-south central greenway

UNIQUE AND DEFINING CHARACTERISTICS

- Legible routes and links to other areas of development, encouraging travel by sustainable modes;
- Attractive setting for local facilities and services;
- Highest density development;
- Contemporary architectural approach;
- Use of increased storey heights (up to 3 storeys);
- Built form positively addressing the Spine Road, primary school and Link Road;
- Mix of commercial and employment uses, with residential units above; and
- Formal hard landscaped multi-functional public space







PUBLIC SPACES. SAFE, SOCIAL AND INCLUSIVE

National Planning Policy Framework chapters 8, 9, 12

“The quality of the spaces between buildings is as important as the buildings themselves. Public spaces are streets, squares, and other spaces that are open to all. They are the setting for most movement. The design of a public space encompasses its siting and integration into the wider network of routes as well as its various elements. These include areas allocated to different users – cars, cyclists and pedestrians – for different purposes such as movement or parking, hard and soft surfaces, street furniture, lighting, signage and public art.”

(Para. 99, NDG 2019)

Green Infrastructure

Natural England defines Green Infrastructure (GI) as a ‘strategically planned and delivered network comprising the broadest range of high-quality green spaces and other environmental spaces.’

This definition has been forefront of the design process and the ambition to create a generous network of multi-functional open spaces within and surrounding the development using the existing features, character and topography as the framework.

12.1 The delivery of well-designed accessible and inclusive public spaces will offer residents spaces to socialise and engage with each other, encouraging interaction and opportunities to benefit from healthy lifestyle choice.

LANDSCAPE STRATEGY

12.2 Landscape design is a key component for creating a successful development at Land east of Willen Road, Newport Pagnell. The proposed green spaces are an integral part of the whole scheme and offer a strong landscape presence across the site.

12.3 Successful public spaces help create more attractive places to live and provide safer routes for users. The following 8 qualities of successful open spaces have been considered in the proposed landscape strategy:

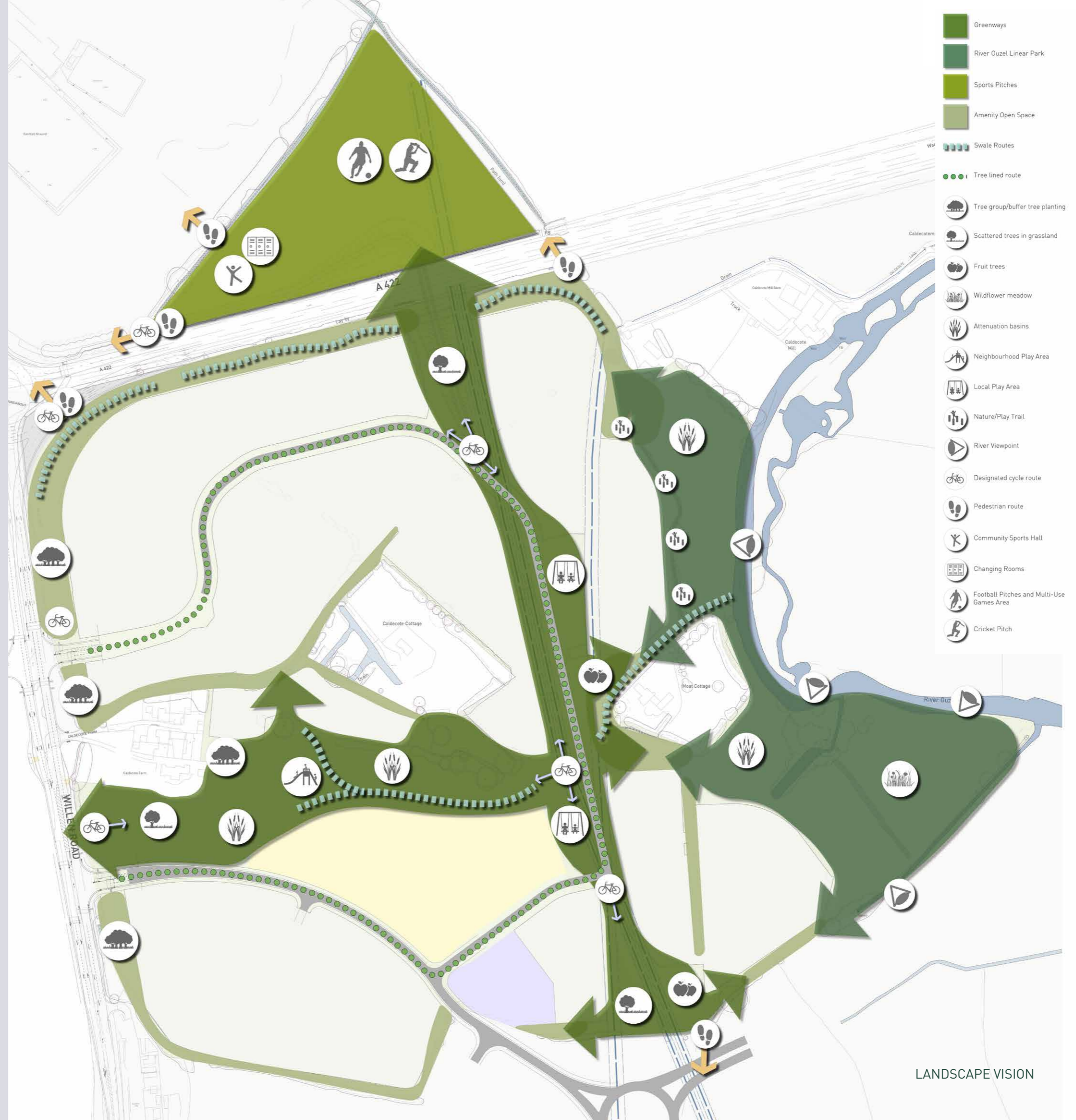
- Sustainability;
- Character and distinctiveness;
- Definition and enclosure;
- Connectivity and accessibility;
- Legibility;
- Adaptability and robustness;
- Inclusiveness; and
- Biodiversity

12.4 The existing site is broadly level, with slightly raised levels in the south west gently falling to the River Ouzel on the east of the site. The River Ouzel flows along the eastern boundary of the site with a portion of the east of the site sitting within the River Ouzel corridor and forming part of the floodplain.

12.5 The existing vegetation forms a network of hedgerows, standard trees and tree groups focused around the site boundary, along the River Ouzel and around the existing properties.

12.6 The site lies within the Landscape Character Types (LCT) River Valley - Area 2D Ouzel North Urban River Valley, as set out within the Milton Keynes Character Assessment (2016). The following guidelines for the River Valley LCT have been considered within the landscape design:

- Encourage the reversion of arable to pasture within floodplain;
- Promote improvements to the river, water edge and pond habitats to encourage biodiversity value through marginal planting and localised bank profiling and sympathetic maintenance of drainage ditches;
- Promote the management of riparian vegetation including floodplain pollards, new specimens including Black Poplar, and where appropriate more extensive areas of wet woodland;
- Encourage the increased use of the river for appropriate recreation that respects landscape character;
- Improve PRow connections from urban and rural settlements to the right of way networks promoting new links where absent;
- Encourage appropriate management of all drainage ditches to improve wildlife value, by improving water quality and establishing grass ‘verges’;
- Consider if there is further scope to minimise the visual impact of the main transport corridors, including the M1 by introducing additional planting;
- Prevent built development in the floodplain;
- Promote the creation of additional public bridging points;
- Improve right of way signage.



LANDSCAPE VISION

To provide a network of high-quality green spaces successfully integrated into the residential development which provide access and movement in a safe and attractive environment. The multi-functional green spaces will endeavour to retain the existing landscape features and deliver a variety of environmental enhancements.

KEY LANDSCAPE PRINCIPLES

- To create a high-quality environment
- To retain and enhance the natural environment and support the existing local landscape character
- To protect, create and support habitats for biodiversity, habitats and species to survive and thrive
- To create a legible and safe series of green spaces for communities to enjoy
- To provide a generous movement network considering accessibility and connectivity, including links to the wider areas
- To respect the flood zone of River Ouzel and create a naturalistic linear park allowing visitors/users to enjoy the River environment
- To encourage healthy active lifestyles through the provision of sports pitches with an associated community building along with a variety of play and recreation opportunities for all ages

12.7 Encompassed within the landscape design will also be the fulfilment of landscape enhancements which have benefits to the landscape character and ecology, which are set out below:

- Provide a buffer zone along the River Ouzel and restore areas of floodplain to semi-natural areas of landscape
- Retention, protection and enhancement of existing habitats through positive management
- Reinforcing existing hedgerows
- Creation of species-rich lowland meadow
- Creation of swales, attenuation basins/ponds with appropriate planting
- Increased shrub and tree/woodland planting, in particular wet-woodland within the outer edges of the floodplain
- Introduce appropriate new species i.e., Black Poplar





KEY		
	Local Park with 600m catchment	1.73 Ha
	Pocket Parks with 300m catchment	1.28 Ha
	Neighbourhood Play Area with 600m catchment	1.16 Ha
	Neighbourhood Play Area active area with 30m offset	
	Local Play Areas with 300m catchment	0.67 Ha
	Local Play Area active area with 20m offset	
	Playing Fields	3.38 Ha
	Overbridge earthworks	1.20 Ha
	Natural/semi-natural green space	8.29 Ha
TOTAL POS PROVIDED		17.71 Ha

NOTE:
All areas given include allowances for existing tree/hedgerow planting, central SuDs corridor, attenuation basins and areas within flood zone 3.

POS BREAKDOWN

OPEN SPACE STRATEGY AND TYPOLOGIES

- 12.8 A number of different types of public open space will be provided throughout the site in order to cater for a range of uses and recreation provision as recommended in both the 'Fields In Trust' Bench Mark guidance and Natural England's Accessible Natural Greenspace Standards.
- 12.9 The Milton Keynes East Strategic Urban Extension Development Framework SPD sets out that 'Open Space should be provided in accordance with guidance set out in Plan MK (Policy L4 and Appendix C). Policy L4 item A sets out that 'New housing development will be required to provide new or contribute to improved open space and recreational facilities in accordance with the Council's adopted standards in Appendix C, the Council's Leisure, Recreation and Sports Facilities SPG and policies in this plan'

- 12.10 While clearly there is a degree of overlap between all the different types of provision, there is a considerable over provision of public open space within the site, which demonstrates a more than adequate provision of both Passive and Active open space within the proposed development. This will include areas ranging from native woodland buffers and blue/green infrastructure corridors to meadows and generous open lawn areas, affording residents a range of spaces to enjoy.

- 12.11 In accordance with Appendix C of the Council's Leisure, Recreation, and Sports Facilities SPG the open space provision will provide the following types of space:
- Local Park;
 - Pocket Parks;
 - Neighbourhood Play Area;
 - Local Play Area;
 - Playing fields; and
 - Amenity Open Space.
- 12.12 The adjacent plans summarises the sizes and locations of the different types of open space.
- 12.13 The below table compares the targets required by Milton Keynes Council and sets out the amount of open space provided within the development.

	Minimum target provision (MK East Development Framework SPD)			On site provision (Ha)*
	Minimum size	Standard per 1,000 population (Ha)	Catchment area	
Local Park	1-2 Ha	not defined	300-400m	1.73
Pocket Parks	up to 1 Ha	not defined	300m	1.28
Local Play Area	0.2 Ha (0.35 Ha if surrounded by housing)	not defined	600m	0.67
Neighbourhood Play Area	0.6 Ha	not defined	400m	1.16
Amenity Open Space	up to 0.1 Ha	not defined	n/a	8.29
Playing Fields	6.4 Ha (para 4.2.26, pg 39)			3.38
Overbridge embankments	n/a	n/a	n/a	1.20
Total Public Open Space Provision				17.71

*All areas given include areas of existing tree/hedgerow planting, central SuDs corridor, attenuation basins, existing utilities and associated easements, and areas within flood zone 3.



LOCAL PARK

12.14 The Local Park is located within the west of the site and will contain the Neighbourhood Play Area and a Local Play Area. It will exceed the Milton Keynes East Strategic Urban Extension Development Framework SPD requirement for size being minimum 1-2ha with the 600m catchment incorporating the entire site. It will also broadly fulfil the principles set out within Plan:MK 2016-2031:

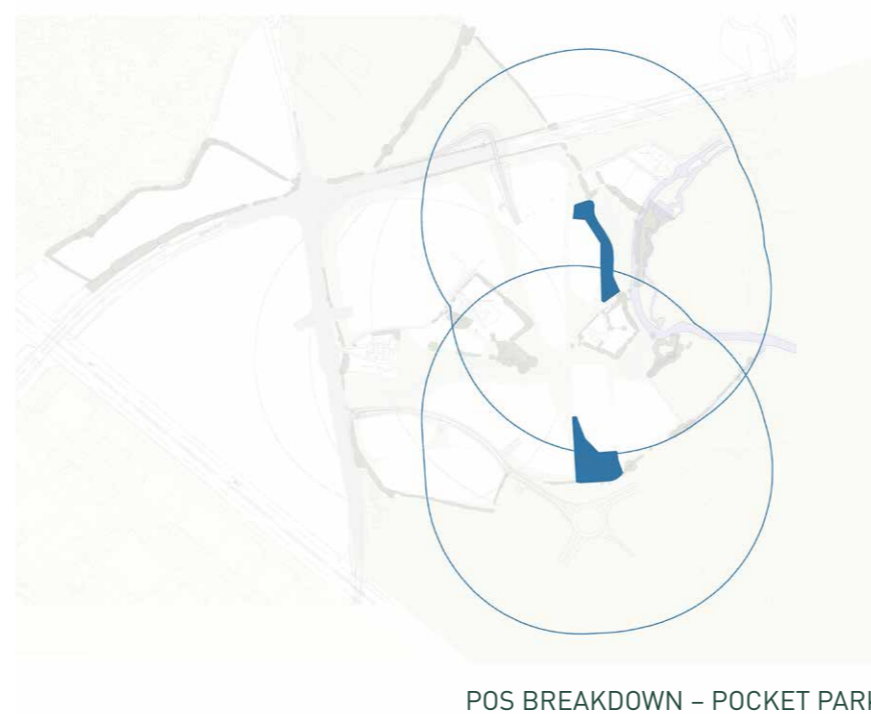
- Form part of the linked network of open space
- Include woodland, natural planting and other wildlife features
- Be in an accessible location and accessible from various locations
- Is anticipated to be accessible via public transport
- Movement routes be overlooked and of a high quality
- Housing to face portions of the park, along with the school property



POCKET PARKS

12.15 Two Pocket Parks are integrated within the site, the first being on the west side of the green space running alongside the River Ouzel, the second is in the centre of the southern boundary of the site. Each space will remain within the maximum size of 1ha set out within the Milton Keynes East Strategic Urban Extension Development Framework SPD and the 300-400m catchment will encompass the majority of the site. The Pocket Parks will also broadly fulfil the principles set out within Plan:MK 2016-2031:

- Form part of the linked network of open space
- Include woodland, natural planting and other wildlife features
- Be integrated into the surrounding development
- Be overlooked by adjacent development
- Be in a highly accessible locations and on key pedestrian routes
- Have direct routes leading to the Pocket Parks that are direct and overlooked



AMENITY OPEN SPACE/OTHER OPEN SPACE

12.16 Remaining areas of open space will be the backbone to the movement network, providing a web of green spaces around and within the development. This public open space category encompasses green spaces that do not fall into the key Plan:MK typologies, including green corridors and incidental areas of open space.





-  Indicative locations of existing trees / tree groups and hedgerows to be retained to BS 5837
-  Feature trees
-  Public open space trees
-  Public open space trees - fruiting
-  Street trees
-  Woodland/copse planting
-  Native structural shrub planting
-  Hedgrow planting
-  Ornamental planting
-  Aquatic/marginal planting to basins
-  Amenity grass
-  Long mown grass
-  Areas of wildflower or grassland meadow
-  Flood zone
-  Attenuation basin/swale
-  Footpath/cycleway
-  Raised timber decking
-  Seating
-  Picnic bench
-  Approximate location of equipped play area, size is indicative
-  Nature play trail
- Green gym items
- Community sports hall with changing rooms
- Sports pitches

STRATEGIC LANDSCAPE MASTERPLAN

LANDSCAPE PROPOSALS

- 12.17 The landscape proposals for the green infrastructure will be broadly naturalistic in character with the strong use of soft landscaping and natural design celebrating the use of native and wildlife attracting plants. Sensitively chosen areas will be designed with a semi-natural/semi-formal character due to their location within the development, reflecting the introduction of built form.
- 12.18 The large open spaces will provide ample opportunities for a variety of recreational opportunities which will include formal and informal play, encouraging the community out into the green spaces to play in and amongst the natural environment.
- 12.19 The significant green elements include:
- **River Ouzel Linear Park** which is a large naturalistic space running along the edge of the River Ouzel. The Linear Park will incorporate a Pocket Park containing an informal nature play/education trail.
 - **East-west greenway**, a linear park running east-west broadly through the centre of the site, a large portion of which is categorised as a Local Park and contains a Neighbourhood Play Area and a Local Play Area.
 - **North-south greenway**, a linear park running north-south broadly through the centre of the site, this greenway incorporates a Local Play Area in the centre and a Pocket Park in the south.
 - A framework of **green corridors** framing the space, softening the boundaries and providing biodiversity connections
 - **Tree lined primary street**



- 1 River Ouzel Linear Park
- 2 East-West Greenway
- 3 North-South Greenway and Central Local Play Area
- 4 Sports Pitches and Community Building
- 5 Green Corridors and Strategic Landscape
- 6 Tree Lined Primary Street

- (a) Attenuation Basin
- (b) Equipped Play Area
- (c) Orchard Planting
- (d) Tree Group/Woodland Planting
- (e) Community Building/Changing Rooms

LANDSCAPE STRATEGY PLAN

12.20 Movement throughout the green infrastructure has been designed to offer the highest level of connectivity and ease of legibility. A network of pedestrian paths weave through the open spaces providing numerous green and pleasant routes for the community to enjoy at leisure whilst ensuring key routes are direct and convenient. Shared pedestrian/ cycleways connect to the existing and proposed Redways in and around the development, extending the opportunities for off-road cycling.



- River Ouzel Linear Park
- East-West Greenway
- North-South Greenway and Central Local Play Area
- Sports Pitches and Community Building

KEY SPACES PLAN

RIVER OUZEL LINEAR PARK

12.21 Running in a north-south direction along the eastern boundary of the site will be a linear green/blue park which will reflect the character of the River Ouzel Park elsewhere in Milton Keynes. The linear park will be naturalistic in character, with the design intending to enhance the biodiversity and landscape value. Community use of the park and interaction with the natural landscape of the River Ouzel will be encouraged through the provision of a network of footpath routes, where required sections of footpath will be raised to offer year-round access to the floodplain. The existing pastoral/arable fields that previously were farmed in the floodplain will be returned to lowland meadow, with scattered trees and small pockets of wet woodland. It is here that appropriate new wetland species, such as Black Poplar could be introduced. A Pocket Park will be provided within the linear park.



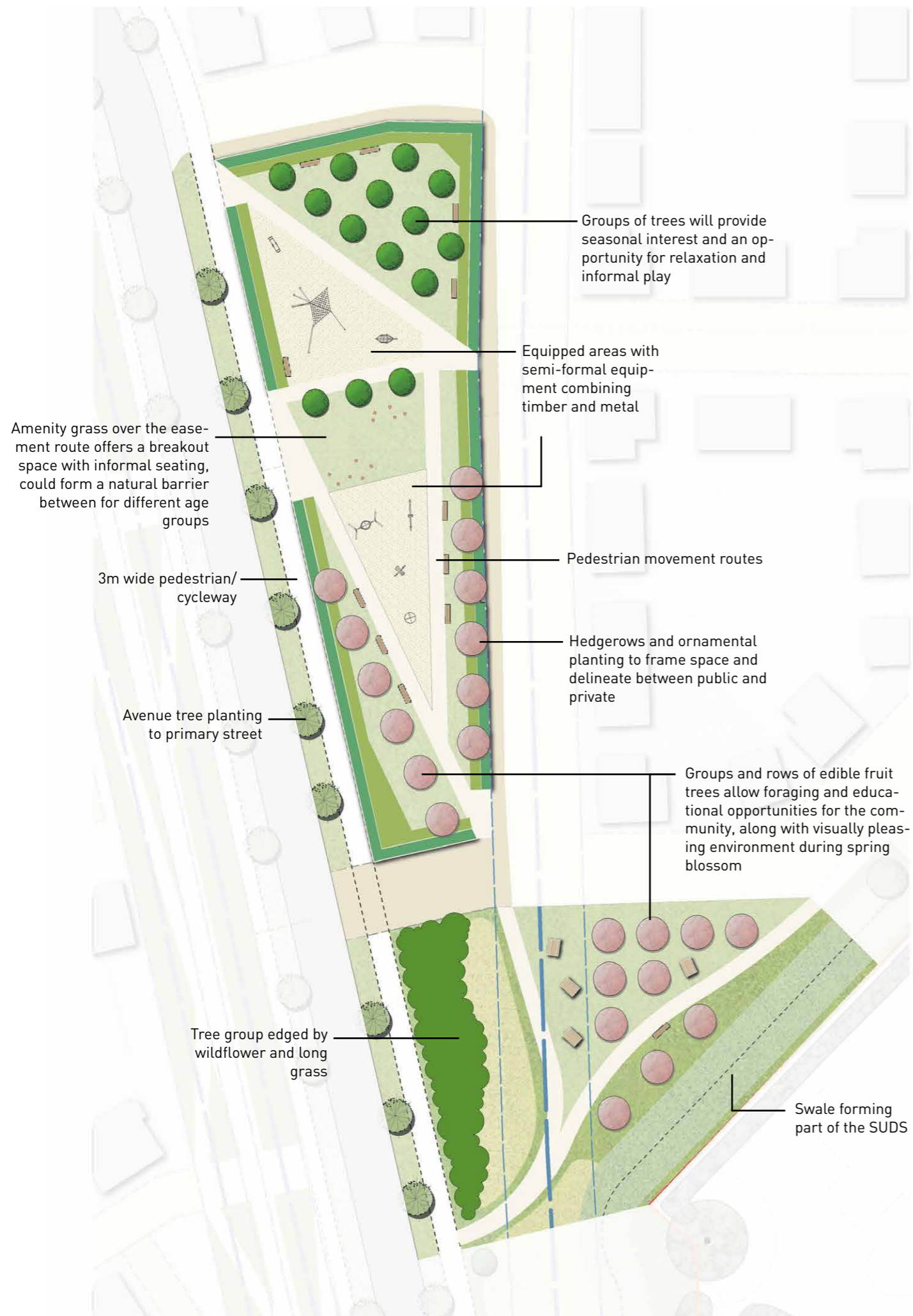
EAST-WEST GREEN CORRIDOR

12.22 Running in an east-west direction through the heart of the site will be a wide green corridor, this key link will provide the Local Park encompassing a Neighbourhood Play Area and a Local Play Area. This highly accessible space will contain numerous pedestrian links and a central pedestrian/cycle route linking the Primary Street within the development to the proposed redway along Willen Road. Swales and basins will flow through the site, extending the 'wetland' character provided by the proximity to the River Ouzel and creating a cohesive green infrastructure.

NORTH-SOUTH GREENWAY WITH CENTRAL LOCAL AREA FOR PLAY

12.23 The North-South Greenway provides a route through the site that offers numerous green and pleasant spaces of varying characters linked through the provision of a pedestrian/cycleway. The northern portion contains the raised landform for the pedestrian overbridge and will take on a naturalistic character. The southern portion opens out to be a wider space with less of a transitional atmosphere, this will be a multifunctional space defined as a Pocket Park with a semi-natural character provided through the juxtaposition of formal footpath lines combined with informal planting such as wildflower meadow and scattered trees. An opportunity to place Green Gym items within this southern area will provide health and recreational opportunities for adults within the natural environment. The central portion of the North-South Greenway abuts the Primary Street so will instinctively take on a semi-formal character with the design introducing more formal shapes and planting styles, it is here on the eastern side that a Local Area for Play will be located, the space will be broadly divided into different zones each offering different uses or characters to allow the space to be enjoyed by a wide variety of users. The equipped play will be semi-formal in character, combining materials such as timber and metal. Hedgerows and ornamental planting will define the space and provide a natural boundary, with edible fruit trees providing a theme to the space and offering educational and community foraging opportunities.





PLAYING FIELDS

- 12.24 A sizeable area has been provided north of the main site for the provision of playing fields, a community building and changing rooms. This area will be accessed via the pedestrian overbridge and from Public Rights of Way links in the wider area. This large area will allow for provision of numerous pitches of varying types to suit requirements. A multi-use games area (MUGA) will be included to provide year-round sports recreation.

STRATEGIC LANDSCAPE AND GREEN CORRIDORS

- 12.25 The site is bounded on the north and west by existing roads, adjacent these generous green curtilages have been designed to retain existing vegetation where possible and to enhance ecological connectivity whilst also providing a visual/noise barrier to the road networks. Planting will be of a semi-natural character expanding from the species mixes using in the other areas of green open space. Along the northern boundary the inclusion of swales will extend the 'wetland' character provided by the proximity to the River Ouzel and help to create cohesive green infrastructure



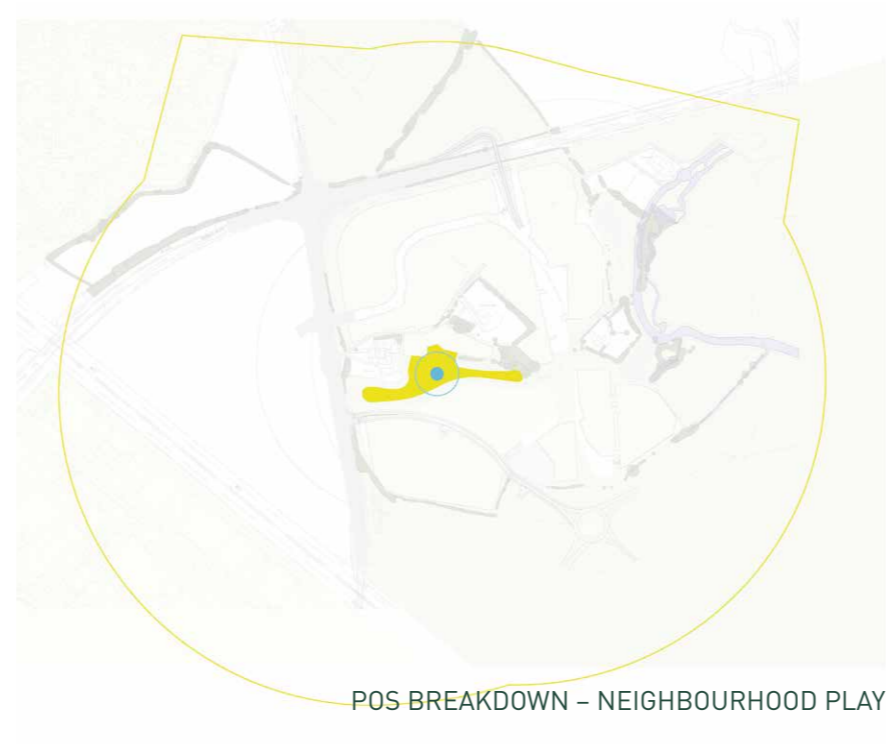
PLAY STRATEGY

- 12.26 Key to the delivery of accessible public open space is the provision of spaces for Children and Young people. A 'playable landscape' approach is proposed within the development, with play opportunities embedded within the site masterplan, with a series of destinations created and joined by a network of footpaths and cycleways.
- 12.27 The open space and play provision set out within the strategic landscape masterplan aims to cater for the recreational needs of the new community in accordance with Plan:MK 2016-2031 and Milton Keynes East Strategic Urban Extension Development Framework SPD. This local policy along with the best practice guidance helps to define and set out the requirements for the overarching play strategy for the scheme and will subsequently help to shape the play spaces at the detailed design stage.
- 12.28 The Play Strategy plan notes the location of the play opportunities across the proposed scheme in conjunction with the other green infrastructure elements which aims to provide a well-connected, accessible and overarching GI framework for the site.
- 12.29 The design of these spaces will be developed in consideration of best practice guidance including the Design for Play: a guide to creating successful play spaces by Play England which provides ideas and practical resources for building new play spaces in a fresh and inspiring manner. The guide advocates a fresh design-led approach to commissioning, based on 10 principles and encapsulated in one golden rule: a successful play space is a place in its own right, specially designed for its location, in such a way as to provide as much play value as possible.
- 12.30 The play and green spaces at Newport Pagnell will aim to achieve the following objectives:
- Designed to enhance its setting;
 - Located in the best possible place;
 - Close to nature;
 - Designed so that children can play in different ways;
 - Geared towards encouraging disabled and able-bodied children to play together;
 - Loved by the community [to help foster a sense of ownership];
 - Where children of all ages play together;
 - Designed to enable children to stretch and challenge themselves in every way;
 - Maintained for play value and environmental sustainability; and
 - Flexible and able to evolve as the children grow.

NEIGHBOURHOOD PLAY AREA

12.31 A Neighbourhood Play Area will be located within the Local Park and will provide a destination play space with various play and recreation opportunities. The Neighbourhood Play Area will satisfy the 0.3ha size requirement as set out within the Milton Keynes East Strategic Urban Extension Development Framework SPD. The Local Play Areas will also broadly fulfil the principles set out within Plan:MK 2016-2031:

- Be in a combined location with a local or pocket park
- Activity zone be min 40m from residential property boundaries
- Will be easily accessible on key pedestrian routes with direct routes to the play area that are overlooked
- For all children but emphasis on 8+ for unsupervised play
- Contain approximately 8 items of play equipment and ball games area and larger more adventurous play equipment



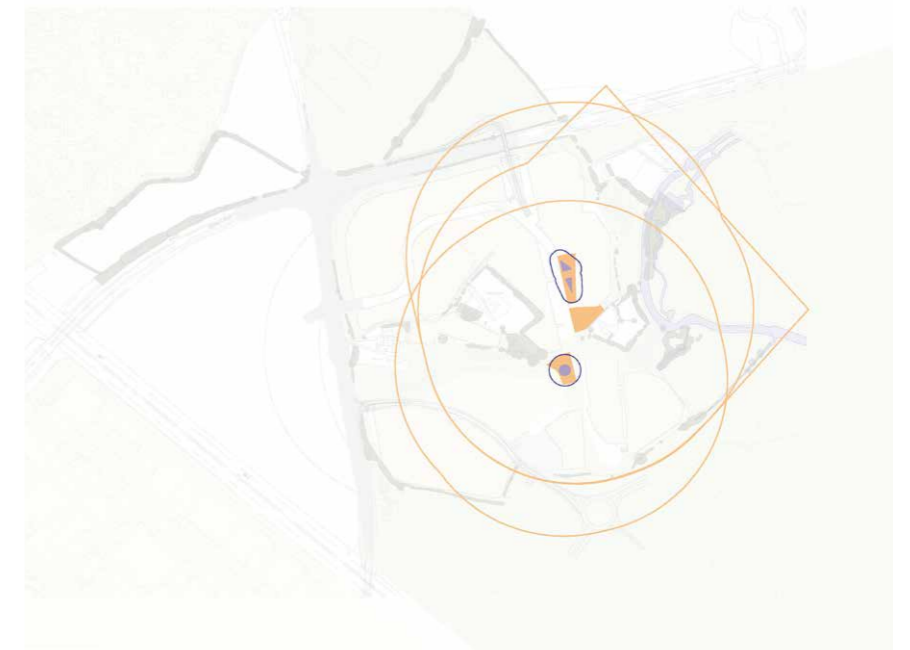


LOCAL PLAY AREAS

12.32 There will be two Local Play Areas located within the development, offering a unique play and recreation experience through interaction with play equipment and natural play opportunities. Each will satisfy the 0.04ha size requirement as set out within the Milton Keynes East Strategic Urban Extension Development Framework SPD. The Local Play Areas will also broadly fulfil the principles set out within Plan:MK 2016-2031:

- Be in a combined location with a local or pocket park
- Activity zone be min 20m from residential property boundaries
- Will be easily accessible on key pedestrian routes
- Be overlooked by the fronts of housing
- Be mainly for children up to age 8 for unsupervised play close to home
- Contain approximately 5 items of play equipment and small games area

12.33 The play areas will be designed in consideration of the existing vegetation and the nature/character of the immediate and local area in which they are located - therefore the Neighbourhood Play Area & Local Play Areas located within the East-West Greenway will be naturalistic in style, and the play equipment be made of timber elements. Their locations should be set against the existing and proposed vegetation to provide shade and attractive space for users.



POS BREAKDOWN - LOCAL PLAY

- 12.34 The Central Local Play Area, due to its urban surroundings, will be characterized by a mixture of both timber and metal equipment so that it complements the semi-formal setting.
- 12.35 To introduce an additional recreation option and connection with the environment, a nature / play trail is also proposed. This trail will provide opportunities to learn through nature or experience risk/challenges in the natural environment, for a wide range of users.
- 12.36 All of the play areas are well connected to the primary pedestrian/cycle path providing opportunity for 'walk and play', therefore the character of the play equipment should be kept in the same style and provide different activity experiences for each locations.
- 12.37 When designing these spaces, it is necessary to take into account the fact that the spaces should also include the necessary infrastructure for caregivers of children, i.e. picnic benches or bicycle stands.
- 12.38 There are a number of best practice guidance documents to be considered in the design of the play and sport provision for the site, these include the Play England guidance, and the Guidance for Outdoor Sport and Play – Beyond the Six Acre Standard, the Fields in Trust - planning and design for outdoor sport and play guidance (2008).
- 12.39 Key considerations and principles for the play spaces (in line with the aforementioned guidance) that shall be taken forward to the detailed design stage are as follows.
- Safety - Designed, manufactured, installed and maintained in accordance with EN1176 and EN1177 in respect of any play equipment.
 - All formal pieces of play equipment will undergo frequent checks to ensure the equipment is safe and fit for purpose, including an independent post-installation inspection. The high-quality appearance of the equipment will be maintained through maintenance inspections with work undertaken as required.

- Safety surfacing to comply with critical fall height requirements as identified by the play manufacturers specification, and compliant with BS EN 11777 and BS7188.
- A variety of play experiences to be included for all ages with a wide range of formal and informal play opportunities set within the open spaces and green corridors.
- Informal opportunities for play and recreation could be incorporated with nature/education trail pieces located within the green corridors. The inclusion of such equipment would help to integrate recreation and fitness activities within the public open spaces. Provision for older years (and potentially adults) could be provided with outdoor gym elements which could further enhance the health and wellbeing benefits provided within the green infrastructure network.
- Mounds as well as tree and shrub planting could be used to enclose the play spaces whilst the earth mounding itself could provide opportunities for play. Mounding to be created with varying gradient to be minimum of 1:6 and maximum of 1:3 to allow planting / seeding and enable management.
- Play equipment to be located outside of root protection areas for retained trees.
- Provision of seating, bins and other appropriate street furniture to be located adjacent to access points and close to paths.
- Where gates are to be provided, they will be self-closing with anti-trap devices.
- Signage to be used within the spaces where relevant to inform users about the play space and it's rules and provide details of whom to contact to report maintenance issues.

PLAYING FIELDS AND OUTDOOR SPORTS FACILITIES

- 12.40 A large area of public playing fields will be included within the development, located north of the site in a previously allocated area. This space will also include a Community Building with changing rooms and a Multi-Use Games Area. The Playing Fields/Outdoor Sports Facilities will also broadly fulfil the principles set out within Plan:MK 2016-2031:
- To have an accessible network of routes around and across the space
 - Pitches must meet Sport England standard



POS BREAKDOWN – PITCHES

NATURE. ENHANCED AND OPTIMISED

NPPF chapters: 8, 12, 14, 15

“Nature contributes to the quality of a place, and to people’s quality of life, and it is a critical component of well-designed places. Natural features are integrated into well-designed development. They include natural and designed landscapes, high quality public open spaces, street trees, and other trees, grass, planting and water.”

(Para 90, NDG 2021)

13.1 Alongside well-designed public spaces the proposed water management and planting strategies offer the opportunity to enhance and optimise the development proposals, providing resilience to climate change and supporting biodiversity.

RETENTION AND MANAGEMENT OF EXISTING VEGETATION

13.2 Across the site much of the existing vegetation will be retained, which includes numerous mixed-species hedgerows, broad-leaved trees groups and scattered individual broad-leaved trees. The retention of these valuable site assets will provide an existing landscape framework, existing habitats and instant visual amenity within the development, from which to build and develop the landscape scheme.

13.3 An understanding of the existing landscape character has influenced the type of landscaping proposed. Species will be mainly native, particularly along the river Ouzel corridor, using a wide mix of plants to increase biodiversity. Plants will be chosen to suit the conditions of the land, most notably to the flood plain, where species will be required to tolerate seasonal flooding. Where planting is closer to the development native plants may be less suitable, in which case wildlife attracting species will be utilised to ensure the biodiversity value of the entire development.

13.4 Particular attention has been to ensure that green networks/ecological routes have been retained and enhanced. Proposed hedgerow, tree group and scattered tree planting has been located to ensure that safe wildlife movement between habitats is maximised. The importance of incorporating street trees, as well as tree planting within other components of the green infrastructure, is reiterated in para 131 of the NPPF 2021:

“Trees make an important contribution to the character and quality of urban environments, and can also help mitigate and adapt to climate change. Planning policies and decisions should ensure that new streets are tree-lined, that opportunities are taken to incorporate trees elsewhere in developments (such as parks and community orchards), that appropriate measures are in place to secure the long-term maintenance of newly-planted trees, and that existing trees are retained wherever possible. Applicants and local planning authorities should work with highways officers and tree officers to ensure that the right trees are planted in the right places, and solutions are found that are compatible with highways standards and the needs of different users”.

(Para 131, NPPF 2021)

13.5 In line with the recommendations made within the ecology report and as a means to fully integrate the public open space as part of the existing landscape, a matrix of meadow grass areas, scrub/shrub planting, native tree planting and fruit tree planting will be established to create varying conditions and enhance biodiversity, with wildflower meadows also created within SuDs features.

13.6 Within the residential curtilage and at the periphery of the site, ornamental grasses will be used to echo the naturalised grassed areas, while allowing for structure and definition to the more formal areas within the residential part of the development.

13.7 More ornamental species and feature tree planting will be used to highlight key areas and nodal points. Placement of trees and choice of species will be to suit the proposed dwellings with species selected to meet National House Building Council (NHBC) requirements.

13.8 There is scope to provide bird & bat boxes (including a barn owl box) within the landscape proposals.

SUSTAINABLE DRAINAGE (SUDS)

- 13.9 The integration of a comprehensive Sustainable Drainage System (SuDs) has been considered from the outset and shaped the masterplan development. The aim of SuDs is to maximise the existing potential of the site to attenuate and clean water, while providing valuable amenity by creating and integrating well designed landscaped features and promoting a greater diversity of flora and fauna. SuDs manage surface water run-off rates by mimicking natural drainage characteristics to achieve a sustainable drainage solution that balances water quality, water quantity, amenity and biodiversity.
- 13.10 Well-designed SuDs also provide opportunities for communities to enjoy the dynamic nature of the water environment and the different habitats that may be sustained by it. The site SuDs has therefore been considered at the outset, with the water management strategy being an integral part of the overall masterplan for the development.
- 13.11 Ponds and wetlands will provide areas for surface water from more severe storms to be accommodated and released at a controlled rate, to the adjacent existing water course. Ponds and wetlands are features with a permanent pool of water that provide both attenuation and treatment of surface water run-off. Attenuation storage is provided above the permanent pool of water, where it is retained for a short period, usually 1-2 days, until it is either taken up by plants, evaporated or slowly released into subsequent features. This will provide an opportunity for the creation of new wildlife habitats and the enhancement of existing ones on site, whilst also presenting recreation and amenity opportunities.
- 13.12 SuDs also include consideration of exceedance of this design standard by accommodating more severe events within the road and landscape areas, and thus preventing flooding of properties and access routes.
- 13.13 The management of all storm water flows up to a 1 in 100-year storm event + 40% (for climate change) will be accommodated within the site, whilst restricting flows to greenfield rates.
- 13.14 The existing surface water flow route on site, which is a result of greenfield run-off along the lower lying areas of the site, has been accommodated within the development, by retaining a green corridor through the heart of the development.
- 13.15 For further information regarding the proposed drainage strategy please refer to the supporting application documentation.



KEY

Dwellings potentially benefiting from solar gain (ie orientated within 30 degrees of south) subject to future detailed design of dwellings

POTENTIAL SOLAR GAIN PLAN

RESOURCES. EFFICIENT AND RESILIENT

National Planning Policy Framework chapters 12, 14

“Well-designed places and buildings conserve natural resources including land, water, energy and materials. Their design responds to the impacts of climate change by being energy efficient and minimising carbon emissions to meet net zero by 2050.”

(Para. 135 NDG, 2021)

- 14.1 The NPPF states at para. 8 that the planning system has three interdependent and overarching objectives:
- An **economic** objective – to build a strong, responsive and competitive economy;
 - A **social** objective – to support strong, vibrant and healthy communities; and
 - An **environmental** objective – protecting and enhancing the natural, built and historic environment
- 14.2 To achieve a sustainable development, that reduces reliance on natural resources and offers a long-term solution for the area the development proposals have been designed with these three key objectives in mind.
- 14.3 At a very high level, the objective of sustainable development can be summarised as meeting the needs of the present without compromising the ability of future generations to meet their own needs. The presumption in favour of sustainable development is at the heart of the planning system, as set out in Para. 11 of the NPPF, and within the Local Development Plan.

LIGHTING

- 14.4 External lighting will be controlled through a combination of movement sensors, time switches and daylight sensors to prevent operation during daylight hours. It will be concentrated in the appropriate areas, and upward lighting will be minimised, reducing unnecessary light pollution, energy consumption and nuisance to neighbouring properties.

SUSTAINABLE BUILDING TECHNIQUES

- 14.5 The proposals will be delivered in line with current building regulations, and where appropriate, will be built with sustainable building construction techniques. Sustainable construction measures could comprise a combination of the following measures:
- Improved energy efficiency through careful building siting, design and orientation;
 - Sustainable Drainage systems (SuDs);
 - Considering fabric efficiency in the design of buildings;
 - Use of building materials capable of being recycled; and
 - An element of construction waste reduction or recycling.

BUILDING REGULATIONS

- 14.6 The proposed development will accord with the very latest building regulation requirements, that emphasise the high levels of building fabric insulation and other materials required to reduce energy and resource requirements. Detailed information regarding the proposed construction methods proposed to achieve buildings regulation compliance will be submitted at the detailed design stage.

MATERIALS AND WASTE RECYCLING

- 14.7 Materials selected for construction, including hard and soft landscaping elements, should be carefully chosen to ensure that they are high-quality, durable and that ‘whole life costs’ are manageable. Sustainable choices will reduce initial manufacturing environmental impacts, long-term maintenance costs and waste from construction, whilst maximising resilience and buildings lifespans.

SITING AND BUILDING ORIENTATION

- 14.8 Dwellings should be carefully sited to ensure that they are sheltered from prevalent winds and benefit from passive solar gain as much as possible.
- 14.9 Passive solar gain can enhance the energy and environmental performance of dwellings. Orientating streets in an east-west direction can increase solar access to dwellings and gardens, whilst avoiding overshadowing from adjacent dwellings. Individual houses which are orientated east of south will benefit from early morning sun, and those orientated to the west of south will benefit from late afternoon sun, which can reduce the need for additional heating during the evening period.
- 14.10 Dwellings/areas of the development that could potentially benefit from passive solar gain or the future installation of solar panels (i.e. are orientated within 30 degrees of south) are identified on the Potential Solar Gain Plan, presented here. The final location and numbers of dwellings benefiting from solar gain will be set out following the design of dwellings at the reserved matters application stage.

LANDSCAPE DESIGN AND MICROCLIMATE

- 14.11 The strategic use of tree planting can mitigate against some of the impact of colder northerly winds. Where possible the development has been designed to be self-sheltering, with arcs of tree planting included to the north-west of the development, to minimise the ‘wind chill effect’ and the potential heat loss from dwellings as a result of strong winds.

SUSTAINABLE COMMUNITIES

- 14.12 The development proposals comprise a range of land uses, providing a good basis for the creation of a sustainable community. Residential development is proposed alongside a mixed-use local centre and primary school, creating a truly walkable neighbourhood. The proposals make effective use of the site, with residential development located adjacent to the existing built form of [insert name], ensuring that the development delivers housing in a location that is both sustainable as well as reducing the reliance on car ownership.
- 14.13 Areas of green space have been incorporated into the proposals with substantial areas of publicly accessible open space, comprising formal and informal amenity open space, play facilities and sports pitches, encouraging opportunities for social and community cohesion. A mix of house types, tenures and sizes are proposed, limiting social exclusion and ensuring the creation of a truly varied and mixed community.

SUSTAINABLE DRAINAGE SYSTEMS

- 14.14 Development has been located away from areas of surface water and fluvial flooding. Surface water run-off rates will be managed by the use of Sustainable Drainage systems (SuDs) on-site, to ensure that the development does not impact on the surrounding area.
- 14.15 Please refer to the Flood Risk Assessment for detailing information regarding the proposed fluvial flood and surface water management strategy for the site.



LIFESPAN. MADE TO LAST

National Planning Policy Framework chapters 8, 12, 14, 15, 16

“Well-designed places sustain their beauty over the long term. They add to the quality of life of their users and as a result, people are more likely to care for them over their lifespan.”

(Para. 151, NDG 2021)

A SENSE OF OWNERSHIP

- 15.1 The proposals create areas that are attractive and with clearly defined public and private areas that relate well with one another to help promote a sense of community identity. The development should enable residents to take pride in their surroundings, which in turn will help create a sense of shared ownership and social responsibility.

ADOPTION AREAS

- 15.2 When completed responsibility for long term management and maintenance will typically be separated into areas including:
- Highway adoption areas;
 - Public open space areas (put forward for local authority or management company maintenance, subject to relevant S106 agreement);
 - Private property ownership; and
 - Shared maintenance areas such as shared private drives.

ADAPTING TO CHANGING CIRCUMSTANCES

- 15.3 The development can potentially accommodate a range of changing needs of the users over time. This includes changes in the health and mobility of the user, as well as potential changes in lifestyle due to developing technologies, such as use of electric vehicles, remote working and general changes to the way in which people live.



CONCLUSION

“Well-designed places and buildings come about when there is a clearly expressed ‘story’ for the design concept and how it has evolved into a design proposal. This explains how the concept influences the layout, form, appearance and details of the proposed development. It may draw its inspiration from the site, its surroundings or a wider context. It may also introduce new approaches to contrast with, or complement, its context. This ‘story’ will inform and address all ten characteristics. It is set out in a Design and Access Statement that accompanies a planning application.”

(Para. 16, NDG 2021)

- 16.1 This Design and Access Statement has set out a clear explanation of the design process, community engagement and consultation process undertaken with the local community and other key stakeholders. The design process has also included a comprehensive and thorough assessment of the site and its immediate context, the development of a clear set of principles to guide the design of the site.
- 16.2 The plans and design approach together with the supporting illustrative strategies demonstrate how the vision for Willen Road, Newport Pagnell can be delivered to meet the 3 key NPPF objectives of sustainable design
- A **social** objective;
 - An **economic** objective
 - An **environmental** objective.

- 16.3 The development of Milton Keynes East provides a unique opportunity to create a new Sustainable Urban Extension, building on the legacy and distinctive character of the site. Creating housing choice and provide areas of truly accessible public open space, whilst improving public access across the site and the wider pedestrian network. The delivery of local facilities and services, alongside development will support both the existing and proposed communities, complementing the existing town of Newport Pagnell and the wider SD12 allocation site.
- 16.4 The masterplan is founded on best practice urban design principles, community integration and sustainable development, with strong links to the wider area.
- 16.5 Willen Road, Newport Pagnell will be a highly desirable place to live for the 21st century and beyond, reflecting the desirable elements of the local vernacular. The proposals respect the local character but also move the community towards a more sustainable future, through a significant increase in housing choice. Development will accord with the principles of high-quality design and best practice to create a townscape that is both varied, and yet sympathetic to its environment. The aim is to achieve a development with a strong identity and distinct sense of place, whilst at the same time integrating with the existing community.

- 16.6 The development proposals will offer the following main benefits:
- The delivery of up to 800 new homes in a range of dwellings types, sizes and tenure, offering an accessible and acceptable choice of lifestyles;
 - The creation of an integrated and sustainable residential community with a sensitive relationship to the existing settlement;
 - Space for the future provision of a new two form entry primary school;
 - Delivery of new open spaces that benefit of both new and existing residents in the area.
 - Providing a development that is well connected, readily understood and easily navigated, with the delivery of new accesses from Willen Road to the west of the site and key strategic links to the wider SUE;
 - The creation of legible routes through the development, complementing existing routes and providing sustainable transport choices;
 - New pedestrian and cycle bridge over the A422, providing a safe off road route to recreational facilities and Newport Pagnell town;
 - The creation of a strong landscape structure, focused around the retained woodland, responding to the local area, and enhancing and optimising the immediate locality; and
 - Delivery of significant areas of open space and new sports pitches to complement the existing Willen Road Sports Ground.

-  DESIGN
-  ENVIRONMENT
-  PLANNING
-  ECONOMICS
-  HERITAGE

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